



CONSOLE RIBS

OWNERS SAFETY MANUAL



For Customer Service or Technical Support visit WALKERBAY.COM

**CALIFORNIA
PROPOSITION 65 WARNING**



WARNING

Operating, servicing and maintaining a recreational marine vessel can expose you to chemicals including engine exhaust, carbon monoxide, phthalates, and lead, which are known to the State of California to cause cancer and birth defects or other reproductive harm. To minimize exposure, avoid breathing exhaust, service your vessel in a well-ventilated area and wear gloves or wash your hands frequently when servicing this vessel.

For more information go to
www.P65warnings.ca.gov/marine.

DECLARACION OF CONFORMITY

International standards to which conformity is declared:
Recreational Craft Directive, RCD: 2013/53/Eu, Module A
International Organization for Standardization, ISO: 6185-3
American Boat and Yacht Council, ABYC: H-28
National Marine Manufacturers Association, NMMA: H-28

CERTIFYING BODIES



IMCI
Langstr. 89
B-4731 Eynatten, Belgium
Tel: 32 87 30 65 30
Fax: 32 87 30 65 36



United States of America
National Marine Manufacturers Assn.
Suite 5100-200 E. Randolph Dr.
Chicago, IL 60601-6528, USA
Tel: 1 (312) 946-6214
Fax: 1 (312) 946-0388

MANUFACTURER

Walker Bay Boats, LLC.
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Seattle, WA 98104
United States
www.walkerbay.com

Customer Service
email: info@walkerbay.com

TYPE OF EQUIPMENT

Inflatable Boat

HULL IDENTIFICATION NUMBER

HIN / WIN (Found on boat): US-EWV_____

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ATTACHMENTS:

MANUFACTURES STATEMENT OF ORIGIN
SPECIFICATIONS & MAXIMUM CAPACITIES
REPLACEMENT WARNING LABEL LIST
RECOMMENDED SEATING ARRANGEMENT
UNAIDED REBOARDING
CONSOLE BASE INSTALLATION GUIDE
ELECTRICAL DIAGRAM
CALIFORNIA EVAP WARRANTY STATEMENT

REGISTRATION AND REGULATION

REGISTER YOUR BOAT (USING YOUR MANUFACTURING STATEMENT OF ORIGIN OR MSO, AND YOUR HULL IDENTIFICATION NUMBER OR HIN) WITH YOUR STATE OR PROVINCE IF REQUIRED. REGULATIONS VARY BY JURISDICTION SO CONTACT YOUR LOCAL REGISTRATION OFFICE DIRECTLY.

The operator is responsible for knowledge of federal, state or local regulations concerning operation requirements. For example: safety equipment, discharge of oil, speed, noise, wake etc.

MANUFACTURERS STATEMENT OF ORIGIN (MSO)

Your MSO should be provided to you by your dealer. Keep your MSO in a safe place. You will need it to register your boat with your state or province. Hand the MSO to the new owner if you sell the boat. If your MSO is lost or stolen contact Walker Bay at info@walkerbay.com for a replacement. Note: There is an administrative fee for issuing replacement MSO's.

WARRANTY AND WARRANTY REGISTRATION

For information on the Limited Warranty and Extended Warranty coverage see the WALKER BAY® CONSOLE RIB LIMITED WARRANTY included with this manual or go to www.walkerbay.com.

IMPORTANT

We strongly recommend that you register your warranty to extended your warranty coverage as defined in the WALKER BAY® CONSOLE RIB LIMITED WARRANTY.

Submit your warranty registration yourself by completing the Warranty Registration/Boat Owner Registration Form online at: www.walkerbay.com/warranty/

Registering your warranty provides Walker Bay with your contact information in case of warranty or service notifications. The Federal Safety Act of 1971 provides for defect notification to the first purchaser. Failure to register your purchase with Walker Bay constitutes waiver of the right to defect notification.

Registering your warranty will speed up your claim and reduce the paperwork needed to process your claim should you require warranty service in the future.



FAQ? *Where do I find the **HIN/WIN** on the boat?*

Answer: The Hull Identification Number or Watercraft Identification Number can be found on the starboard/right outside of the transom of your boat.



FAQ? *What **model year** is my boat?*

Answer: The model year is the last two digits of your HIN/WIN which can be found on the starboard/right outside of the transom of your boat.

INTRODUCTION

Congratulations on your purchase of a Walker Bay Rigid Inflatable Boat (RIB). Walker Bay RIBs are the benchmark in luxury console RIBs. They combine elegant styling with quality materials and components. With their modified, variable V fiberglass hulls they provide a smooth ride and easy planning. The spacious interior of their wide-body designs are extremely comfortable and afford generous space for crew and gear.

Welcome to the future of luxury console RIBs

ABOUT THIS OWNER'S SAFETY MANUAL

This manual has been compiled to help you to operate your craft with safety and pleasure. It contains details of the craft; the equipment supplied or fitted, its systems and information on their operation. Please read it carefully and familiarize yourself with the craft before using it.

This Owner's Safety Manual is not a course on boating safety or seamanship. If this is your first craft or inflatable, or if you are changing to a type of craft you are not familiar with, for your own comfort and safety, please ensure that you obtain handling and operating experience before "assuming command" of the craft. Your dealer or national boating/sailing federation or yacht club will be pleased to advise you of local sea schools or competent instructors.

Ensure that the anticipated wind and sea conditions will correspond to the design category of your craft (see DESIGN CATEGORIES) and that you and your crew are able to handle the craft in these conditions. Even when your boat is categorized for them, the sea and wind conditions corresponding to the design categories A, B, and C range from severe storm conditions for category A, to strong conditions for the top of category C, open to the hazards of a freak wave or gust. These are therefore dangerous conditions, where only a competent, fit and trained crew using a well maintained craft can satisfactorily operate.

This Owner's Safety Manual is not a detailed maintenance or trouble-shooting guide. In case of difficulty, contact Walker Bay or its national representatives.

Always use trained and competent people for maintenance, fixing or modifications. Modifications that may affect the safety of the craft must be assessed, executed and documented by competent people. Walker Bay or its representative cannot be held responsible for modifications that Walker Bay® has not approved. Modifications or attachment of items to the hull or tube may affect your warranty (see LIMITED WARRANTY).

In some countries a driving license, boating license, certification, or authorization are required, or specific regulations are in force for operating a marine craft. Check with your local boating safety office.

Always maintain your craft properly and make allowances for the deterioration that will occur over time and as a result of heavy use or misuse of the craft. Any craft, no matter how strong it may be, can be severely damaged if not used properly. This is not compatible with safe boating (see SAFETY INFORMATION). Always adjust the speed and direction of the craft to sea conditions.

The craft should have onboard the appropriate safety equipment (see SAFETY EQUIPMENT) according to the type of craft, weather conditions etc. This equipment is mandatory in some countries. The crew should be familiar with the use of all safety equipment and emergency maneuvering (see SAFETY EQUIPMENT & EMERGENCY PROCEDURES). Contact your local boating/sailing organization for information, lessons or drill sessions.

All persons should wear a suitable Coast Guard approved buoyancy aid (life jacket/personal floatation device). Note that in some countries, it is a legal requirement to wear a buoyancy aid that complies with their national regulations at all times.

PLEASE KEEP THIS OWNER'S MANUAL AND YOUR MSO IN A SECURE PLACE AND HAND IT TO THE NEW OWNER WHEN YOU SELL THE CRAFT.

STATE OF CALIFORNIA EMISSION REQUIREMENTS

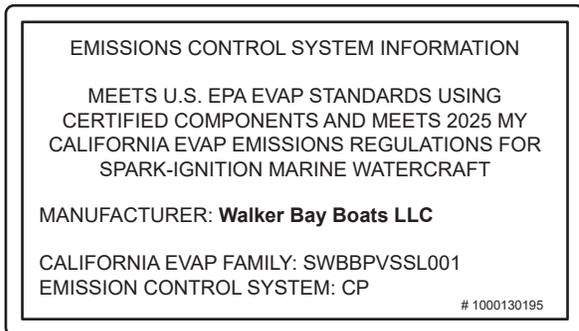
Your boat may be equipped with an engine that meets the special requirements of the California Air Resources Board (CARB). If so, the engine is designed to comply with strict requirements and will have one of the following labels. These labels are required by CARB. The label will display 1, 2, 3, or 4 stars and must be affixed to your engine if it will be operated in the state of California or in bordering waters. For more information, visit: <http://www.arb.ca.gov>



FUEL SYSTEMS

Boats manufactured for use in California for model year 2018 and after meet the California EVAP Emissions regulation for spark-ignition marine watercraft. Boats meeting this requirement will have a label affixed near the helm.

The fuel system in this boat complies with U.S. EPA mandated evaporative emission standards at time of manufacture using certified components. See the label below.



Note: the YYYY MY (model year) along with the California Evap Family first Letter in the Family grouping changes annually in this label.

For detailed information regarding your warranty rights and obligations under the California Air Resources Board (CARB) regulations, please refer to the "California Evap Warranty Statement" provided as a separate document with this manual.

PROPOSITION 65

⚠ WARNING

A wide variety of components used on this vessel contain or emit chemicals known to the State of California to cause cancer and birth defects and other reproductive harm.

EXAMPLES INCLUDE:

- Engine and generator exhaust.
- Engine and generator fuel, and other liquids such as coolants and oil, especially used motor oil.
- Cooking fuels.
- Cleaners, paints and substances used for vessel repair.
- Waste materials that result from wear of vessel components.
- Lead from battery terminals and from other sources such as ballast or fishing sinkers.

TO AVOID HARM:

- Keep away from engine, generator and cooking fuel exhaust fumes.
- Wash area thoroughly with soap and water after handling the substances above.

1.- SAFETY INFORMATION

BOATING SAFETY SIGNS AND SYMBOLS



This is the safety alert symbol. It is used to alert you to potential personal injury hazards. Obey all safety messages that follow this symbol to avoid possible injury or death.

 DANGER
Indicates an imminently hazardous situation which, if not avoided, will result in death or serious injury or substantial property damage.

 WARNING
Indicates an imminently hazardous situation which, if not avoided, could result in death or serious injury or property damage.

 CAUTION
Indicates an imminently hazardous situation which, if not avoided, may result in property damage.

CAUTION
Used without the safety alert symbol indicates a potentially hazardous situation, which, if not avoided, may result in property damage.

NOTICE

Indicates installation, operation or maintenance information which is important but not hazard related.

GENERAL INFORMATION

According to the Recreational Craft Directive 2013/53/EU, your boat has design category: C, i.e., the craft is designed to operate in the following conditions:

Wind (Beaufort scale) up to and including Force 6;

Significant wave height ($H^{1/3}$, meters) up to and including 2 meters.

Explanations of design categories are given below:

DESIGN CATEGORIES

A - Ocean - A craft given design category A is considered to be designed to operate in winds of less than Beaufort force 10 and the associated significant wave heights.

NOTE: Typically such conditions might be encountered on extended voyages, for example across oceans, but can also occur inshore when unsheltered from the wind and waves for several hundred nautical miles. Depending on atmospheric conditions, winds can gust to about 32 m/s.

B - Offshore - A craft given design category B is considered to be designed to operate in winds of Beaufort force 8 or less and the associated significant waves heights of up to 4 m.

NOTE: Typically such conditions might be encountered on offshore voyages of sufficient length but can also occur on coasts where shelter might not always be immediately available. These conditions can also be experienced on inland seas of sufficient size for the wave height to be generated. Depending on atmospheric conditions, winds can gust to about 27 m/s.

C - Inshore - A craft given design category C is considered to be designed to operate in typical steady winds of Beaufort force 6 or less and the associated significant waves heights of up to 2 m.

NOTE Typically such conditions might be encountered on exposed inland waters, in estuaries, and in coastal waters in moderate weather conditions. Depending on atmospheric conditions, winds can gust to about 18 m/s.

D - Sheltered waters - A craft given design category D is considered to be designed to operate in typical steady winds of Beaufort force 4 or less and the associated significant waves heights of up to 0,3 m and occasional waves of 0,5 m height.

NOTE Typically such conditions might be encountered on sheltered inland waters, and in coastal waters in fine weather. Depending on atmospheric conditions, winds can gust to about 12 m/s.

MAXIMUM NUMBER OF PERSONS

The maximum number of people (adults + children) allowed on board according to ISO 14946 design standards is detailed in the sheet attached (See SPECIFICATIONS AND MAXIMUM CAPACITIES).

MAXIMUM LOAD CAPACITY

You can find the maximum load capacity in the sheet attached to this manual (See SPECIFICATIONS AND MAXIMUM CAPACITIES). Please note that the maximum load capacity includes the weight of all recommended persons onboard, all provisions, personal effects, any additional equipment minus liquids in Fixed tanks.

Important Note: The maximum number of people and the maximum load capacity can be read on the manufacturer's plate attached to the boat.



WARNING

Do not exceed maximum recommended number of persons and maximum recommended load. Regardless of number of persons on board, total weight of persons and equipment must not exceed maximum recommended load. Always use seat(s) / seating spaces provided.

Always load craft carefully and distribute loads appropriately to maintain design trim (approximately level). Avoid standing or placing heavy weights high up.

LOADING - BOARDING

 WARNING
<p>Do not exceed maximum recommended number of persons.</p> <p>Do not exceed maximum weight capacity. Regardless of number of persons on board, total weight of persons and equipment must not exceed maximum weight capacity.</p> <p>Step or climb into boat. Do not jump into boat. Board one person at a time. Load gear after boarding.</p> <p>Load and board craft carefully. Distribute loads to maintain design trim (approximately level). Avoid placing heavy weights high up or standing.</p> <p>Sit inside boat. When motoring all passengers should sit inside boat or on seat(s) provided, not on tubes. Passengers sitting on tubes can fall overboard.</p> <p>Use handholds provided. When operating boat all passengers should use handholds provided. Passengers not using handholds can fall overboard.</p> <p>Children and non-swimmers must wear lifejackets or PFDs when boarding and when onboard.</p>

 CAUTION
<p>Inspect and monitor onboard loads to prevent chafing or puncture of tube.</p>

LOADING - PASSENGERS

Board the vessel with caution. To review the recommended seat positions, please refer to the sheet attached to this manual titled "Recommended Seating Arrangements."

All passengers, including children, must wear a life jacket approved by the U.S. Coast Guard or by the appropriate local authorities.

EMERGENCY PROCEDURES

Helmsmen must be familiar with the use of all safety equipment and emergency procedures (e.g., man overboard recovery, towing, use of fire extinguishers, etc.). We strongly recommend that they receive proper training from a recognized boating or safety organization before operating the vessel.

Some emergency procedures are described below:

 WARNING
<p>PUNCTURE OR LOSS OF PRESSURE TO AIR CHAMBER</p> <ul style="list-style-type: none">• Shift weight to opposite side of boat (use caution when shifting weight).• Secure leaking or deflated chamber by tying or holding up.• Immediately proceed to nearest safe land. <p>LOSS OF POWER</p> <ul style="list-style-type: none">• Drop anchor.• Attempt to fix problem (carry motor owner's manual, spare parts/tool kit).• Signal for help.• Stay with boat. <p><i>(continued on next page)</i></p>



WARNING

(continued from previous page)

PERSON OVERBOARD PROCEDURE

- Throw life jacket.
- Come around into wind and waves.
- Cut engine when alongside person.

RE-BOARDING WITHOUT CAPSIZE OR SWAMPING

- Turn motor off for re-boarding. Do not approach transom or motor while motor is running.
- Use handles for re-boarding.
- Use caution when assisting re-boarding passenger.
- Distribute weight while re-boarding to prevent capsizing.
- To avoid falling overboard, when operating boat do not stand, sit inside not on tubes, and use handles.

SWAMPING

- Bail boat using a bailer or,
- Open drain plug and motor. Water should drain out when boat is moving forward. Close drain plug when water has drained.

ACCIDENT REPORTING (USA)

Obtain knowledge of accident reporting requirements for the United States Coast Guard. Copies of the U.S. Coast Guard Boating Accident Report are available by calling the U.S. Coast Guard Boating Safety Hotline at 1 (800) 368-5647.

RENDERING ASSISTANCE (USA)

United States Code, Title 46 states that "The owner operator of a vessel is required by law to render assistance to any individual or vessel in distress, so long as his vessel is not endangered in the process."

U.S. Coast Guard Boating Safety Hotline:
1 (800) 368-5647 or 1 (202) 267-1070

STABILITY AND BUOYANCY

The stability and buoyancy have been thoroughly assessed in compliance with the requirements of the Recreation Craft Directive 2013/53/EU, ensuring guarantees that the boat meets all safety criteria during normal operating conditions.

However, the boat operator must bear in mind that the safety of the boat and its crew must be their priority at all times and that conditions on the water can change suddenly.

ENGINE INFORMATION

Information about recommended and maximum engine capacities and maximum engine weights for each model are given in the attached technical data sheet of "SPECIFICATIONS AND MAXIMUM CAPACITIES".

Important Note: Routine engine maintenance must be performed to ensure proper operation and reliability. Owners should always follow the maintenance procedures, service intervals, and safety instructions provided in the engine manufacturer's owner's manual, as these contain the most accurate and model-specific requirements.



WARNING

Do not overpower.

Do not use a motor with a higher horse power, or kilowatts than the builder's plate states.

Overpowering can result in severe handling or stability problems.



CAUTION

Do not under power. A motor too small for boat size or load may not have adequate power to navigate currents or headwinds.

Do not use a motor not approved for use with the boat.

RISK OF FLOODING AND STABILITY

The drainage system on most models consists of deck drainage (See fig. 1) and hull drainage, water entering the deck is drained through deck drains into the bilge and the bilge pump (see Bilge PUMP SYSTEM) directs the water outside the boat.

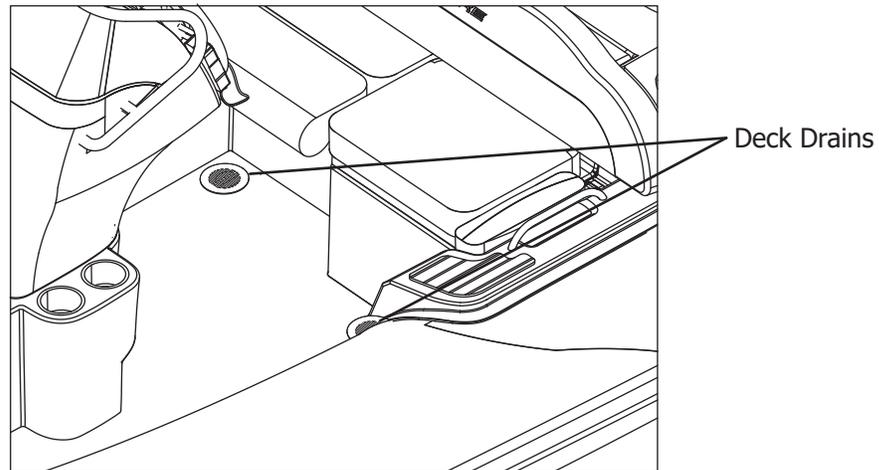


Fig. 1. Deck drainage.

HULL DRAINAGE

The drain plug is not self-bailing.

To drain water from the hull, unscrew the plug (See fig. 2). Screw the plug back in before launching the boat.

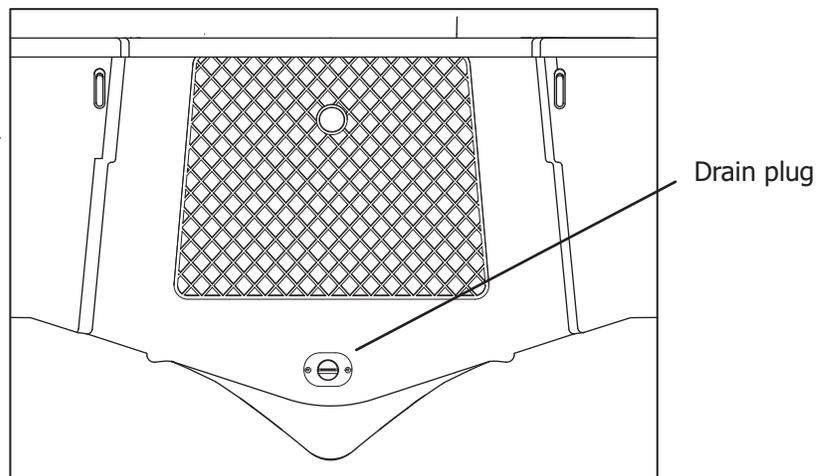


Fig. 2. Hull drainage.



CAUTION

Water accumulation from rain can cause damage to internal components such as the battery.

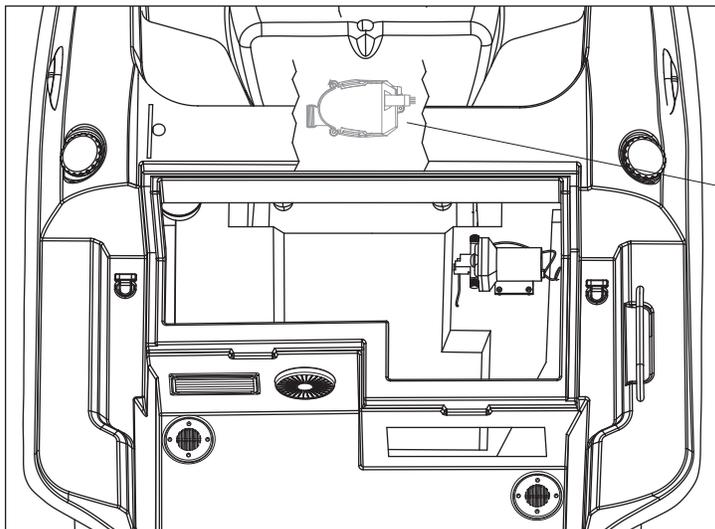
When the boat is on land/davit, floating dock or trailer, always keep the drain plug open so that water can drain from the boat.

BILGE PUMP SYSTEM

The automatic bilge pump serves to extract water from the bilge to the outside of the boat.

Access the bilge pump for maintenance purposes through the rear compartment lid (See fig.3).

The bilge pump can be turned on/off manually by a switch located on the console dashboard and automatically by a float type switch.



Bilge pump



WARNING

The bilge pumping system is not designed for damage control, i.e., for the rapid accumulation of water due to adverse weather conditions, hull damage, or other unsafe navigation conditions.

Fig. 3. Location of the bilge pump.

BILGE PUMP SPECIFICATIONS

ABYC Specifications 13.6-volts DC
(GPH= Gallons Per Hour)

Capacity 498 GPH (at zero head).
Power 12 V/3 AMP
Maximum head 6.7 ft.
Connection 3/4".
Overall dimensions 6-1/16 x 3-1/2 x 4-1/16 (LxWxH) inches.

ISO Specifications 12.0-volts DC
(LPH= Liters Per Hour)

Capacity 1530 LPH (at zero head).
Power 12 V/3 AMP
Maximum head 1.6 m.
Connection 3/4".
Overall dimensions 155 x 88 x 103 (LxWxH) mm.



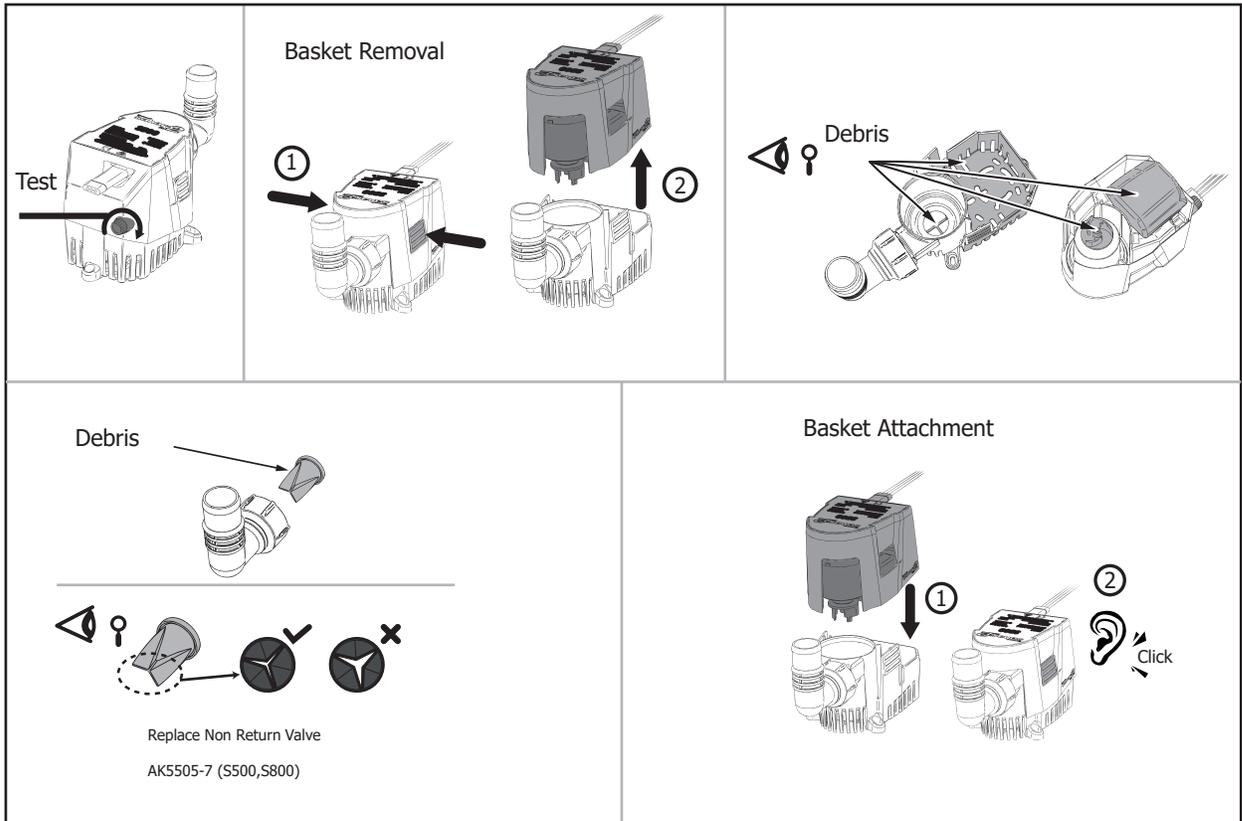
CAUTION

Debris in the inlet can damage the bilge pump.

Clear debris from the pump inlet.

Check the operation of the bilge pump regularly.

MAINTENANCE



RISK OF FIRE OR EXPLOSION

Please note that your boat has a fuel system and electrical components installed, which means there is a risk of explosion or fire caused by a spark. Be careful and check systems regularly in order to identify damage to the fuel line and possible damage to the electrical wiring.

FIRE PREVENTION

The owner/operator of the boat is responsible for:

- Informing all crew members about the location and operation of fire-fighting equipment.
- Check fire-fighting equipment at the intervals indicated on the fire-fighting equipment.
- Replace the used fire-fighting equipment with one of the same type and rating.



WARNING

Never block access to safety equipment such as the fire extinguisher or the main switch for the electrical system.

Never alter the boat's electrical or fuel systems or allow an unqualified person to modify any of the boat's systems.

Never fill the fuel tank or handle fuel when the engine is running.

Never smoke or use a naked flame when handling fuel.

FUEL SYSTEM

The boat has an internal fuel system.

Fuel system consists of the following components (Fig. 4):

- built-in fuel tank (1) with level sensor
- fuel line (from fuel fill neck to fuel tank) (2)
- fuel fill neck (3)
- ventilation line (4) with ventilation fitting (5)
- carbon canister (6)
- fuel line to engine from fuel filter (7) (Installed by dealer, not manufacturer)
- fuel filter/water separator (8)

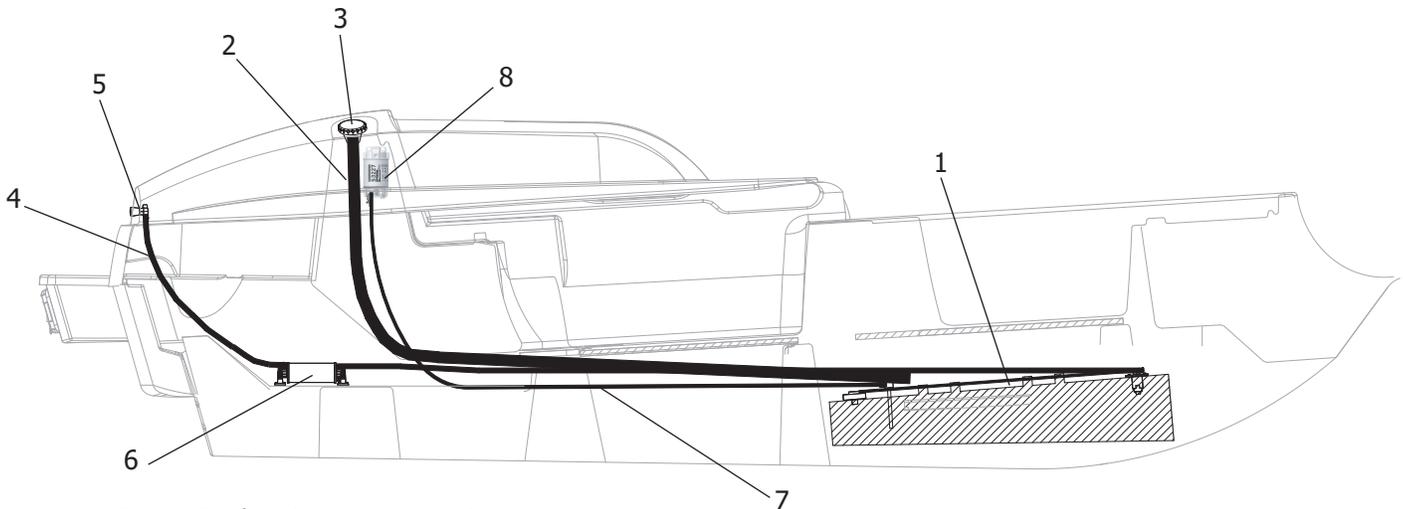


Fig. 4. Fuel system components.

 WARNING
Danger of fire or explosion. Be extremely careful when operating the fuel system, e.g. refueling, installing, uninstalling or cleaning. Fuel fumes may ignite from a static spark and cause serious injury and damage. Do not smoke, allow open flame, or spark within 15 meters of fuelling location.

Fuel Tank

The fuel tank is located below the deck. The capacity of the tank installed in the boat varies depending on the model and can be found on the "SPECIFICATIONS AND MAXIMUM CAPACITIES" sheet attached to this manual.

Important Note: The usable capacity is slightly lower than the maximum capacity specified by the tank manufacturer. This difference exists for safety reasons: fuel, especially gasoline, expands when heated. The empty space inside the tank allows for this expansion, preventing damage, deformation, or cracking caused by thermal expansion, particularly during refueling.

This space also ensures proper ventilation. Fuel tanks require a venting system that allows air to enter and exit the tank, preventing vacuum formation as fuel is consumed.

Fuel Vent

The fuel tank ventilation is provided by a carbon canister connected through the vent line. The VaporTec fuel pressure management system, (fuel fill deck fitting, integrated check valve, fill limit vent valve, grade valve), ensures that the fuel system constantly maintains proper vapor pressure in all situations.

This prevents any unintended pressure that could cause serious damage to the boat or engine. Additionally, the fuel ventilation system plays a key role in controlling the "FULL" fuel level through the use of the FLVV.

Level valves have been added to the tank to allow proper ventilation when the boat is stored or towed on moderate inclines, preventing fuel leaks.

Carbon Canister

The carbon canister connected to the fuel tank's vent line operates similarly to automotive systems. As fuel enters the tank, displaced air containing gasoline vapors is routed through the canister. The activated carbon inside traps these vapors, reducing emissions and preventing fuel odors from escaping into the atmosphere.

The carbon canister rarely requires replacement and is not a serviceable or repairable component. It has a limited lifespan. Replacement decisions are made by qualified technicians during routine inspections of the boat's fuel system.

Ventilation fitting

MAINTENANCE

You will know the vent needs maintenance if vapor rushes into or out of fuel tank when you open deck fill cap. You may also notice difficulty while fueling (back-splash or pump shutting off continuously during fill).

Debris and spider webs can block vent, especially after storage. Use these steps to clean vent screen before and after storage, or at least once a year.

Do this maintenance while boat is out of the water, so you do not risk losing parts.

1. Use a 3/16" (5mm) hex wrench (allen wrench) to turn cap off counter-clockwise. Set cap aside for re-installation. (Figure 5)
2. While cleaning, DO NOT allow debris to fall down the vent tube into tank.
3. Remove screen and clean with solvent or air pressure.
4. Replace screen in original position.
5. Replace and tighten vent cap (30-to-35 in·lb. torque, 3.39 to 3.95 N·Meters).

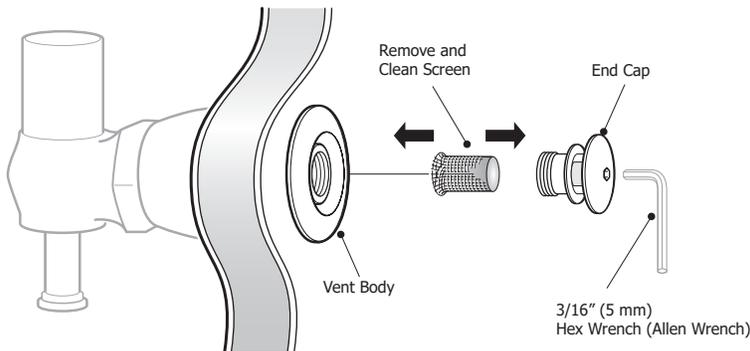


Figure 5. P-trap Vent.

Filling the tank

This fuel system is designed to automatically shut-off the fill nozzle when the tank is full, similar to an automotive fuel system. The automatic shut-off system ensures the operator a clean and trouble-free refueling process. Attempting to fill the tank beyond this point may cause certain components to malfunction or fail. (See refueling).

Fuel Distribution System

Fuel is supplied from the tank to the engine through the pickup tube, the anti-siphon valve (ASV), and the fuel line.

The anti-siphon valve allows fuel to flow only when required by the engine. This safety device is designed to prevent fuel from draining by gravity from the tank if the fuel line is cut or ruptured below the tank level.

Fuel Filter/Water separator

The fuel filter removes moisture and impurities from the fuel supply before it enters the engine's fuel system.

It should be inspected periodically according to the engine manufacturer's instructions.

Never use automotive-type fuel filters on your boat. Dispose of all fuel residues safely and in an environmentally responsible manner.

Filter Replacement

Filter replacement frequency is determined by contamination levels in fuels. Fuel flow to engine becomes restricted as filter slowly plugs with contaminants, resulting in noticeable power loss and/or hard starting. Replace filter annually, every 500 hours, or when there is a noticeable power loss, whichever comes first. Change filter as soon as possible. Note: always carry extra replacement filters as one tankful of excessively contaminated fuel can plug a filter.

Clean any debris or dirt away from Racor head/filter joint prior to removal.

REFUELING:

- Before refueling, stop the engine.
- Do not attempt to fill the tank after the automatic shut-off; overfilling the tank may cause the fuel system to malfunction.
- Refuel in permitted refueling areas, such as fuel docks in the harbour or gas stations. These locations include precautions that lower the probability of static explosion.
- Read all warnings on the fuel distributor and around. Act accordingly.
- Do not board the boat while refueling. Boarding and disembarking may create a static charge that may ignite the fumes over the intake pipe.
- When filling for the first time, and during successive refueling, check the entire fuel system of the boat for leaks or damage. If any leaking or damaged part is found, repair immediately, and ventilate the area to remove explosive fumes.
- Dry spilled fuel thoroughly. Dispose of rags on-shore, according to regulations.
- Before starting the engine or any equipment, check for fuel fumes.

 WARNING
Avoid serious injury or death.
Leaking fuel is a fire and explosion hazard.
Inspect system regularly. Examine fuel system for leaks and corrosion at least once annually.

VENTILATION OF FUEL TANK COMPARTMENT

The fuel tank compartment has a natural ventilation system.

The compartment is ventilated by the natural flow of air through ventilation openings to the atmosphere. These vents are typically located in the rear compartment and the forward storage compartments. (See Fig.6, 7, 8).

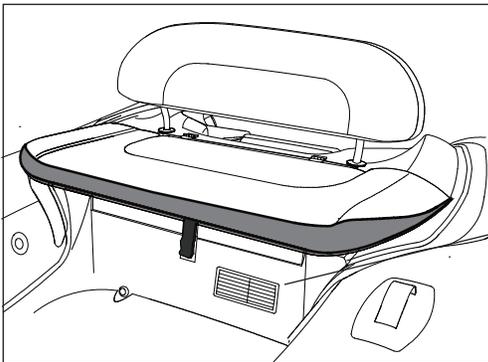


Fig.6. Ventilation opening on the rear compartment.

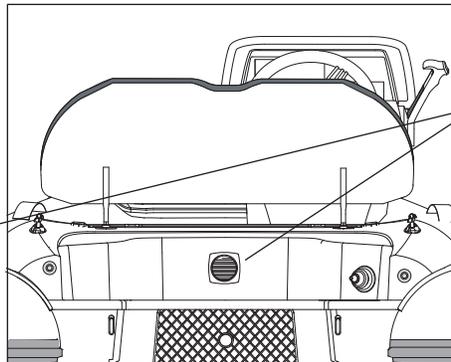


Fig.7. Ventilation opening on the rear compartment.

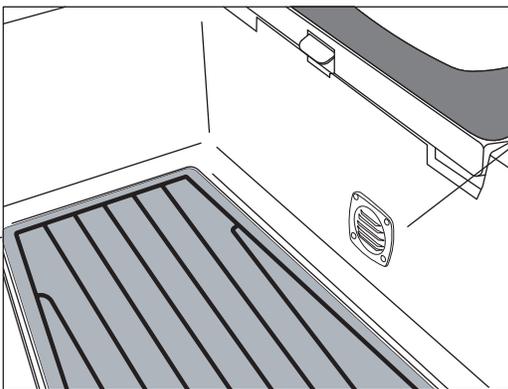


Fig.8. Ventilation opening on the forward storage compartment.

 WARNING
Do not obstruct or modify the ventilation openings. Removing or altering an opening reduces the efficiency of the natural ventilation system (for example, removing an opening to install a speaker).
The ventilation openings must remain clear and unobstructed at all times.

FIRE-FIGHTING EQUIPMENT

It is the responsibility of the Owner/Operator to have fire extinguishers on board in accordance with local requirements. There must be at least one portable type 34 B extinguisher (EU Market) on board.

U.S. Market

For boats 16 ft or less: at least 1 USCG 5-B type fire extinguisher.

For boats over 16 ft and less than 26 ft: at least 2 USCG 5-B type fire extinguishers.

Fire fighting equipment must be easily accessible.

Some models feature a storage box located on the port side, outside the rear storage compartment (see Fig. 9).

Other models do not have a storage box; however, it is recommended to install the fire extinguisher inside the rear storage compartment (see Fig. 10).

Important Note: Portable fire extinguisher storage compartments are marked with the symbol shown in Fig. 11.

fire extinguisher
storage box

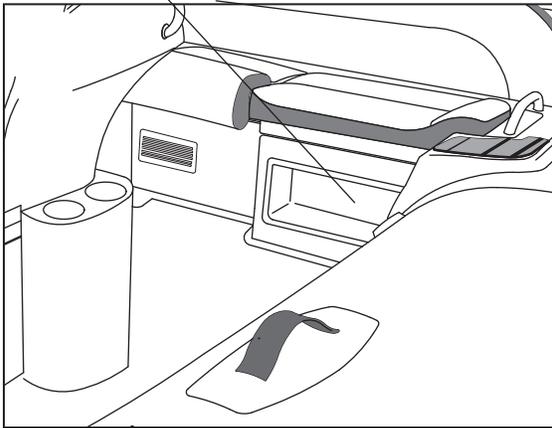


Fig. 9. Storage box for a fire extinguisher.

fire extinguisher

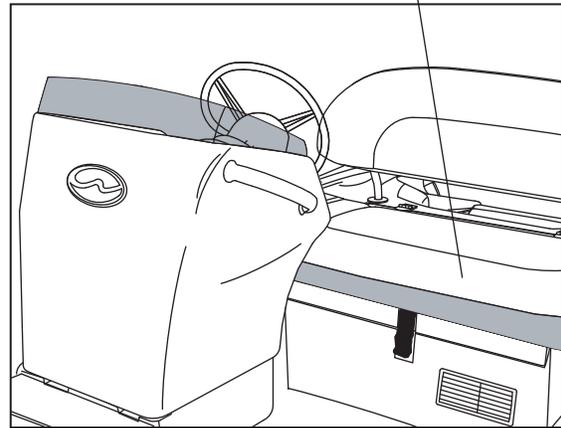


Fig. 10. Recommended location inside the rear storage compartment.



Fig. 11. Symbol of designated place of portable fire extinguisher.

 WARNING	
Never block access to portable fire extinguishers.	
Never use water to extinguish burning fuel or electrical wiring. Using water can spread the liquid and make the fire worse.	
If the fire is small, use a fire extinguisher or fire blanket over the flames.	
If the fire starts to get out of control, before all passengers are forced to leave the boat, check that everyone is wearing a lifejacket.	

ELECTRICAL SYSTEM - RISKS OF FIRE, EXPLOSION OR ELECTRIC SHOCKS



DANGER

The risks of fire or explosion may result from improper use of electric DC.



WARNING

Never work on the electrical installation while the system is energized;

Never modify the craft's electrical system. Installation, alterations and maintenance should be performed by a competent marine electrical technician;

Never alter or modify the rated current amperage of overcurrent protective devices;

Never install or replace electrical appliances or devices with components which exceed the rated current amperage of the circuit;

Never leave the craft unattended with the electrical system energized, except automatic bilge pump.

BATTERY

If the boat is not used for a long time or during the winter period, recharge the battery at least once a month to prolong its life.

Flooded lead, AGM, gel-cell and maintenance free battery types are approved for use in the boat.

If an unsealed battery is used, take care to check periodically the electrolyte level. Specific information on battery care can be found in the battery manual.

Some models include two batteries, one for starting the engine and one for service (provides power to other appliances and equipment on the boat).

When replacing a battery, never mix types (for example, an AGM with a flooded-lead battery). All batteries installed on your boat must be of the same type.

Important Note: The manufacturer does not include the battery with the boat, the dealer is the one who adds the battery upon on the boat delivery.

Typically in most models the battery is located inside the steering console, in other models the battery may be inside the rear storage compartment on the port side.



WARNING

Be careful handling the battery because the electrolyte is acid. If your skin or other body parts are exposed to the electrolyte, clean with fresh water and seek medical assistance.

Be careful when connecting or disconnecting the battery. Do not charge the battery in the boat.

Keep the battery terminals clean, tight and isolated to prevent arc discharges, short circuits, explosion, or fire.

Keep good ventilation in the battery compartment. Make sure that the battery is securely fastened.

MAN-OVERBOARD PREVENTION AND RECOVERY

The working deck has an anti-slip surface that comes from the molding of the boat deck.

When the boat is underway, the passengers should always occupy designated seating areas (See Recommended Seating Arrangements), using the handles attached to the inflatable tube. Other parts such as the cleats and console handrail can be used as handholds to reduce the risk of falling overboard.

 WARNING
<p>If passengers do not remain seated when underway there is a risk of falling inside the boat or overboard causing serious injury or death.</p> <p>Do not make sharp turns at high speed, you could be thrown from the boat.</p> <p>Do not make sudden changes in direction without informing passengers.</p>

MEANS OF REBOARDING

In the event that a passenger falls overboard, there are several ways to re-board a Walker Bay boat from the water. Some models are equipped with ladders to facilitate re-boarding.

For the boarding procedure with or without a ladder see attached sheet "Unaided re-boarding".

LIFERAFT STOWAGE (CE MARKET)

Life rafts for design categories C and D are not considered part of the essential safety equipment.

However, for boats with a length greater than 6 metres, they must have one or more life rafts up to the limit of passengers.

Use the rear storage compartment to store the life raft(s), soft bag life rafts are recommended (not rigid canister) for easy storage in the compartment.

SAFETY EQUIPMENT

The Owner/Operator is responsible for ensuring that appropriate safety equipment is onboard and is in proper working condition.

 CAUTION	
<p>Carry appropriate safety equipment* such as but not limited to:</p> <ul style="list-style-type: none">- Coast Guard approved lifejackets or personal flotation devices for each passenger**- charts- tool kit/ spare parts kit- flares- fire extinguisher- anchor- waterproof flashlight- tow line(s)- pump- oars/paddles- EPIRB device- protective clothing in case of weather change- whistle- first aid kit- navigation lights- mobile phone- tube repair kit- bailer	
<p>*Note: Specific safety equipment is mandatory in some countries. Failure to carry safety equipment onboard could result in fines or other penalties. Crew should be familiar with use of all safety equipment and emergency maneuvering (re-boarding, towing, etc.). Contact your local boating or sailing organization for information, lessons or drill sessions.</p>	
<p>**Note: All persons should wear a Coast Guard approved buoyancy aid (life jacket/personal flotation device). In some countries, it is a legal requirement to wear a buoyancy aid at all times that complies with national regulations.</p>	

NAVIGATION

 WARNING	
<p>Do not operate the boat under the influence of drugs or alcohol or any other impairment or disability.</p>	

 CAUTION	
<p>Observe right-of-way as defined by "Rules of the Road" and required by COLREGS.</p>	
<p>Obey signs for restricted areas designated for swimming, bathing or other forms of recreation.</p>	
<p>Learn to control your boat and know the limitations of your capabilities and the capabilities of your boat. If necessary take a boating course to learn these limitations.</p>	
<p>Inform someone of your Float Plan including time and place of departure, proposed route and expected time of return.</p>	

2. BOAT IDENTIFICATION

BOAT'S IDENTIFICATION NUMBER

Every boat has a "Watercraft Identification Number" (WIN) or "Hull Identification Number" (HIN), which uniquely identifies your boat and is important for safety and identification purposes. The WIN or HIN is marked on a metal plate, which is fixed to the starboard side of the transom of your boat (See fig. 12).

Important Note: In some models, the HIN may be fixed on the starboard side, near the upper edge of the boarding step, to keep it visible.

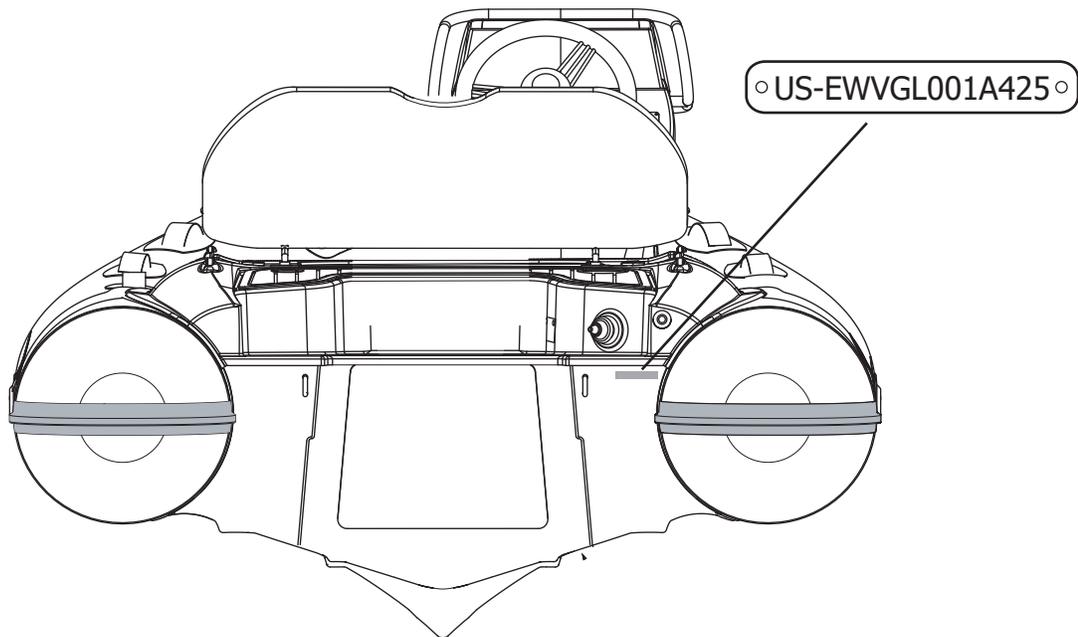


Fig. 12. Location of the WIN or HIN plate.

BUILDER'S PLATE

The builder's plate (See fig. 13) is always located close to the steering position of the boat, provides information about the builder including builder's name, CE marking, design category, maximum recommended load, maximum number of persons.

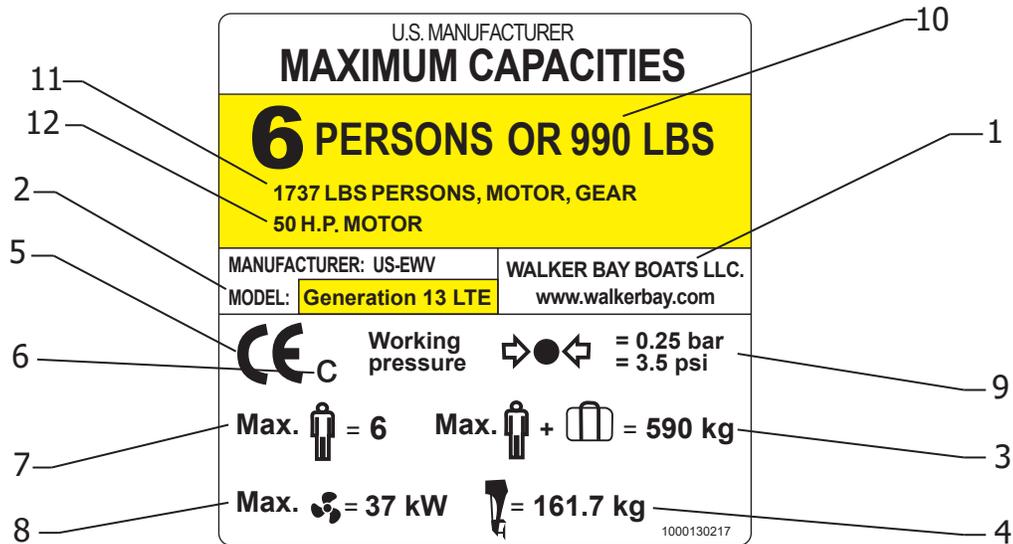


Fig. 13. Sample Builder's Plate.

Important Note: This is a builder's plate that includes maximum capacity information for European market and the US market (filled in yellow).

- 1 - Manufacturer's name
- 2 - Model name
- 3 - Maximum recommended load (including mass of the persons and cargo onboard, but excluding mass of the max motor(s), the mass of the contents of fixed fuel and water tanks when full)
- 4 - Maximum mass of the outboard(s) including controls and batteries
- 5 - CE marking
- 6 - Design Categories according to Directive 2013/53/EU
- 7 - Maximum recommended persons' capacity (75 kg each)
- 8 - Maximum outboard(s) power rating (in kW)
- 9 - Nominal pressure for each inflatable buoyancy chambers

Only US Market

- 10 - Maximum persons capacity in whole number of persons and in pounds
- 11 - Maximum load capacity (total weight on board including persons, motor(s), and gear)
- 12 - Maximum power capacity in horsepower

LOCATION OF THE BUILDER'S PLATE

The builder's plate (See fig. 14) is always located close to the steering position of the boat, provides information about the builder including builder's name, CE marking, design category, maximum recommended load, maximum number of persons.

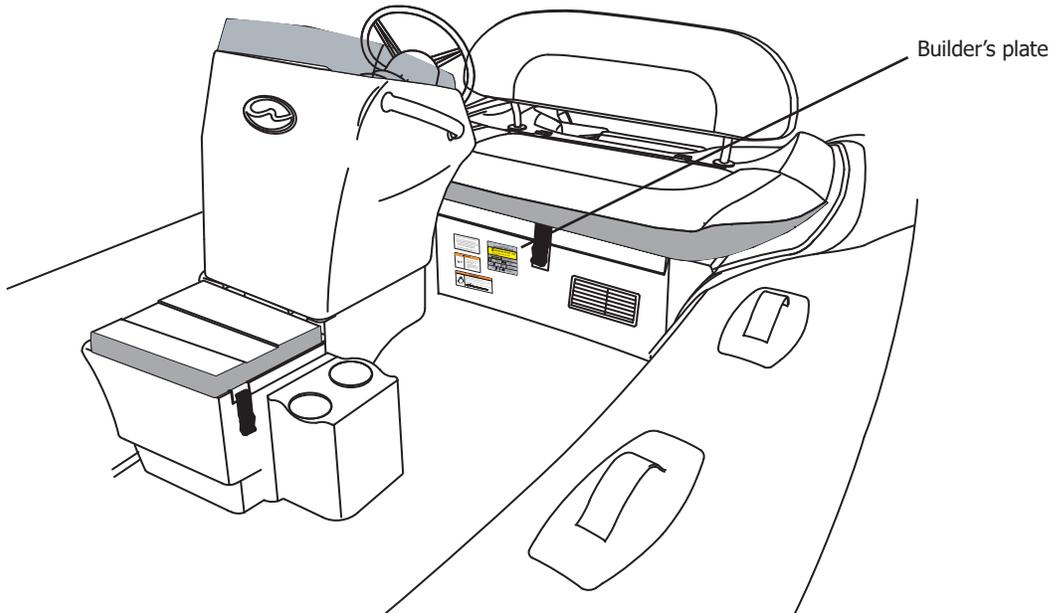


Fig. 14. Location of the builder's plate.

3. BEFORE OPERATING YOUR CRAFT

CHECKLIST BEFORE OPERATING



CAUTION

Perform checklist actions before operating.

- Documents including registration and licenses on board.
- Federal/state/local regulations concerning operation requirements and safety equipment.
- Weather forecast.
- Local area and conditions.
- Inform someone of your intended route and return time.
- Surrounding area for swimmers or divers.
- Surrounding area for hazards such as submerged rocks.
- Tube pressure is correct and valves are securely fastened (see INFLATION & VALVES).
- Remove the hull drain plug, check the gasket, and replace it if necessary. Make sure the drain plug is securely closed. (see FEATURES & DECK PLANS).
- Fuel lines are intact and connections are secure.
- Inspect the steering system: an improperly maintained system may fail, causing sudden loss of control.
- Adequate fuel for the return journey. Consider extra fuel for delays due to weather or currents.
- Engine is securely fastened to motor mount and gear lever is in "Neutral" position.
- People and cargo are correctly and evenly distributed and do not exceed maximum capacity.
- Propeller is clear of obstructions.
- Safety equipment on board (see SAFETY EQUIPMENT).

WEATHER AND HAZARDS

All boats in this owner's manual are ISO design category "C" Inshore - Designed for voyages in coastal waters, large bays, estuaries, lakes and rivers where conditions up to, and including, wind force 6 and significant wave heights up to, and including, 2m. may be experienced.



WARNING

BEWARE OF OFFSHORE WINDS AND CURRENTS

Check weather forecasts and obtain local knowledge of marine environment and weather conditions prior to and during voyage.

Carry appropriate safety equipment (see SAFETY EQUIPMENT). Marine weather can change quickly.

Use extreme caution when motoring in darkness. Turn on your navigation lights in low light conditions. Check with local marine authorities on regulations on navigation lights.



CAUTION

Wrecks, reefs, rocky shores, sandbars, and shallows are hazardous and should be avoided or approached with extreme caution.

4. OPERATING THE CRAFT

INFLATION

First time inflation - When inflating your boat for the first time, inflate all air chambers evenly as directed in INFLATION SEQUENCE. Once the boat is completely inflated release 50% of the air pressure in all chambers and re-inflate all chambers to working pressure. This will allow fabric to settle from packaging.



CAUTION

Do not use a compressed air source (ie. automotive tire air compressor) to inflate boat/tubes. Over-inflation by a compressed air source can rupture seams and/or bulkheads.

When using an electric pump to inflate your boat, finish inflation procedure with manual foot pump provided.

Air chambers must be inflated evenly and to correct working pressure (0.25bar/3.5psi).

Over a period of days tubes may lose pressure. Check pressure before use and re-inflate chambers to working pressure if necessary. Failure to do so could result in chafing or damage to tube or other parts of boat.

INFLATION SEQUENCE

Using foot pump provided, inflate each chamber to approximately 50%. This will protect the baffles which separate the chambers from unnecessary distortion and potential harm.

Finish inflating each chamber. The tubes should be "drum tight". Working pressure of tubes is 0.25bar or 3.5psi. Use a pressure gauge (not supplied) to check and regulate the pressure in the tube chambers to the working pressure.



CAUTION

Do not exceed working pressure of tubes. Exceeding working pressure can damage fabric, baffles, or seams and cause loss of pressure.

CHANGES IN PRESSURE - ALTITUDE



CAUTION

Changes in air temperature will affect air pressure in tubes. Monitor air pressure in tubes and adjust to maintain working pressure.

Changes in elevation or altitude will affect air pressure. In case of changes of elevation or altitude, adjust air pressure in tubes and/or floor to maintain working pressure. To prevent over-inflation, decrease air pressure before transporting boat to higher elevation.

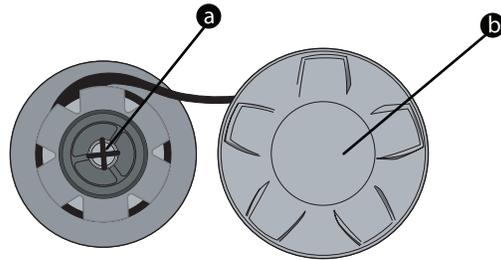
OPERATION OF VALVES

If this is the first time inflating tube, check that valves are tight. Use tool provided in repair kit and gently tighten valves.

INFLATING - To retain air when inflating, release valves to "out" position by pushing center button (a) and turning approximately 90 degrees counter clockwise.

Secure valve with "cap" (b) when finished inflating.

DEFLATING - To release air from tubes remove "cap" (b) and position valve in "in" position by pushing center button (a) in and turning approximately 90 degrees clockwise. Valve should remain in the "in" position.



CAUTION

Do not force valves. They may break.

MOTOR INSTALLATION

The dealer is responsible for installing the motor on your boat.

Read Owner's Manual provided with motor carefully before using the engine and boat.

MOTOR TRIM

Trim motor properly. Correct trim is essential to optimum motoring performance. A general rule is the axis of the propeller should be parallel to surface of water.

Motor trimmed too positively: boat may porpoise or ride "bow high".

Motor trimmed too negatively: boat may ride nose down and have difficulty planing.



CAUTION

Motoring into wind: adjust trim down and keep weight forward to prevent possible capsize.

Motoring in same direction as waves or swell, adjust trim up and keep weight back to prevent bow from submerging.

MOTORING



WARNING

Risk of injury. Propeller can cut, injure and kill.

Avoid moving parts of engine and propeller.

Use an emergency switch or lanyard attached to your wrist. This will stop motor when lanyard is pulled. Check motor shuts off when lanyard is pulled. The stop/kill switch should activate.

Do not motor near swimmers or divers.

Do not allow swimmers to approach stern or propeller while motor is running.



CAUTION

Risk of capsizing, loss of control or falling overboard.

Do not make sharp turns at high speeds. Sharp turns can cause loss of control. See SPECIFICATIONS AND MAXIMUM CAPACITIES for maximum recommended speeds.

Shift engine gear lever to neutral before starting. Unexpected movement can make occupants fall overboard.

Before starting motor, check motor bracket screws are securely fastened. Loose motor bracket screws can result in loss of control or loss of motor.

Pay attention to winds, water conditions, currents and tides. They can affect fuel consumption.

Allow sufficient distance to stop or maneuver if required to avoid collisions.



CAUTION

Reduce speed in waves. Avoid excessive speeds and sharp turns in rough weather.

Avoid rapid acceleration. Rapid acceleration can make operator or passengers fall overboard.

Do not reverse quickly. Reversing quickly can cause swamping.

ELECTRICAL SYSTEM

The boat is equipped with a 12V electrical system, powered by the battery. An electrical switch panel on the center console distributes the power to the given components. The direct current system supplies all circuits on the boat, including navigation lights, pumps, courtesy lights, usb power socket, ignition, etc.

The battery must be secured on the boat and terminals covered. Terminals must be kept clean and checked regularly.

BATTERY ISOLATOR SWITCH

The battery isolator switch isolates the battery from the boat's electrical circuits.

The battery isolator switch should be in the off position when the boat is not in use or will be stored for a long period to prevent battery discharge.

The battery isolation switch is located on the starboard side inside the rear storage compartment or beneath the console. The switch can be identified by its red knob (See fig.15).

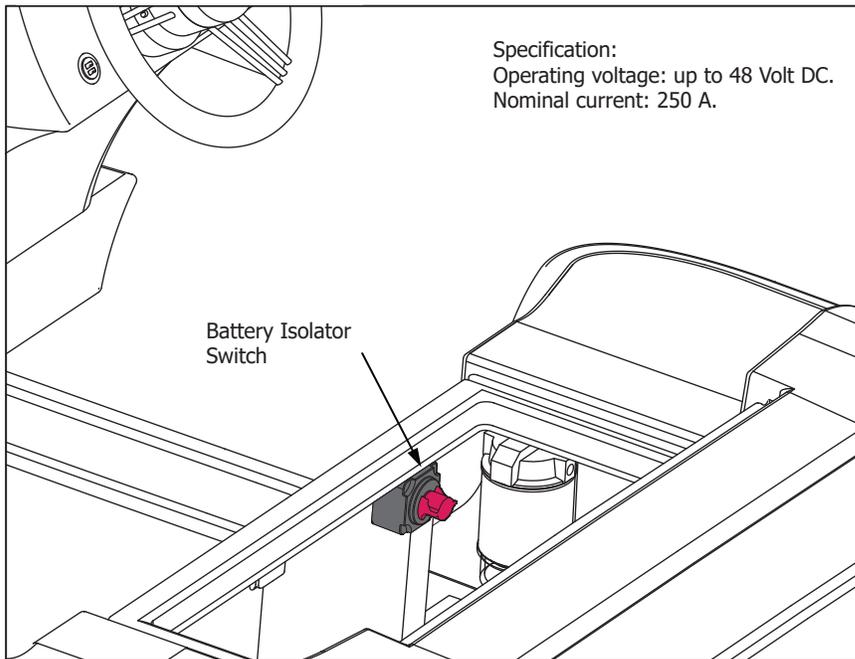


Fig. 15. Example location of battery isolator switch.



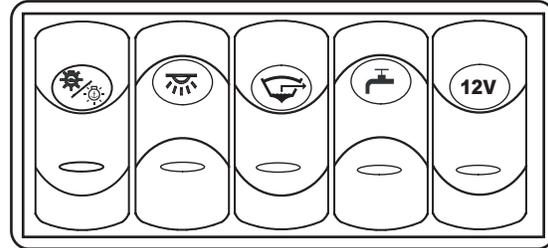
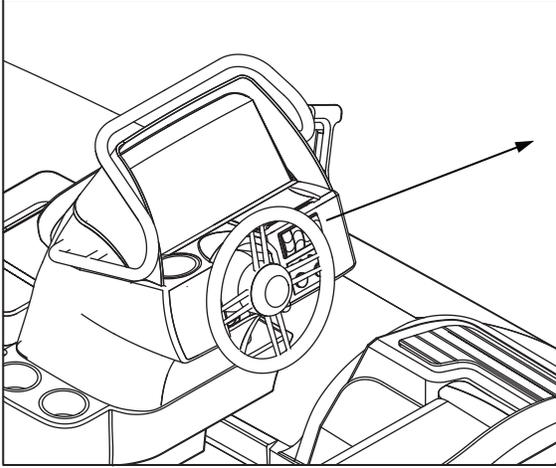
CAUTION

The battery must never be disconnected from the engine when it is running!

SWITCHES

Depending on the model of your boat you may have a switch panel of 2 to 5 switches located on the steering console dashboard (See fig. 16).

Use the switches to turn on/off the navigation lights, interior lights, bilge pump, shower and USB charger and 12V Outlet.



Navigation lights Interior lights Bilge pump Shower 12V Outlet

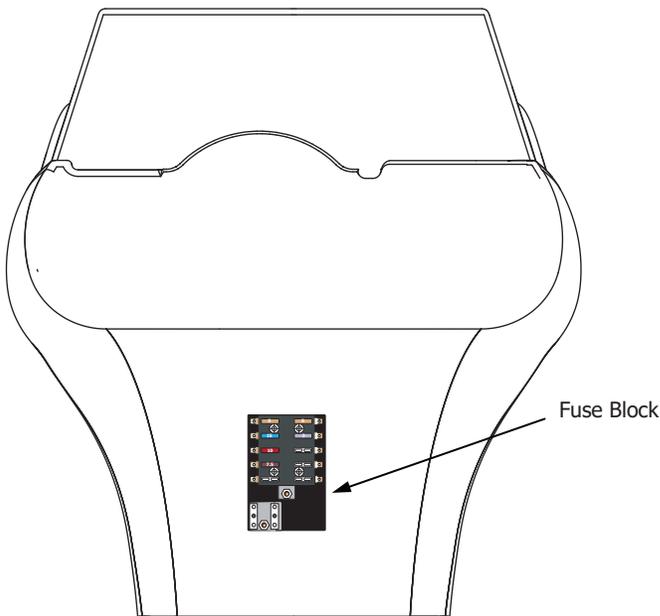
Fig. 16. Switch panel.

FUSE BLOCK

The fuse block is located inside the steering console (See fig.17). To access the fuse block, unlock the console latch and fold the console forward.

Important Note: If the steering console does not fold forward, access the fuse block by removing the console backrest cushion.

To replace a damaged fuse, for your safety first turn off the battery switch. Once the system is de-energized, remove the blown fuse and install a new fuse of the same type and rating.



Circuit	Fuse rating, A
Navigation lights	5
Interior Lights	
Bilge pump	3
Shower	7.5
USB / 12 V Outlet	10
Extra Fuse	15

Fig. 17. Example location of the Fuse block.

NAVIGATION LIGHTS

Your boat is equipped with navigation lights.

Some models feature either a bi-color light or separate side lights; both types are installed on the bow step.

The green light is illuminated towards the starboard (right) side and the red light is illuminated towards the port (left) side.

To install the folding all-round pole light in the stern, slide the cap of the mount fitting aside.

Then gently insert the pole light mast into the slot and screw on the locking collar (the electrical connection will be made automatically).

To provide 360° lighting.

All navigation lights are controlled by the switch panel on the console.

 CAUTION
Do not use the boat between sunset and sunrise, or in low-light conditions, without turning on navigation lights. The lights are important means for boaters to provide current location and to indicate direction of movement.

It is the operator's responsibility to ensure that the navigation lights are functioning properly and that the lighting is appropriate and does not obstruct the intended arc of visibility.

While Navigation lights have an expected service life of over 50,000 hours, their performance can degrade over time due to exposure to the marine environment, vibration, and natural aging of components.

Each lamp is marked with an "LED Expiration Date" (See fig. 18) which is determined by the manufacturer to ensure that the light maintains the minimum visibility range required by regulations (typically 2 nautical miles).

Therefore, each LED navigation light must be replaced no later than the expiration date indicated on the lamp housing, regardless of whether it still functions or emits light.

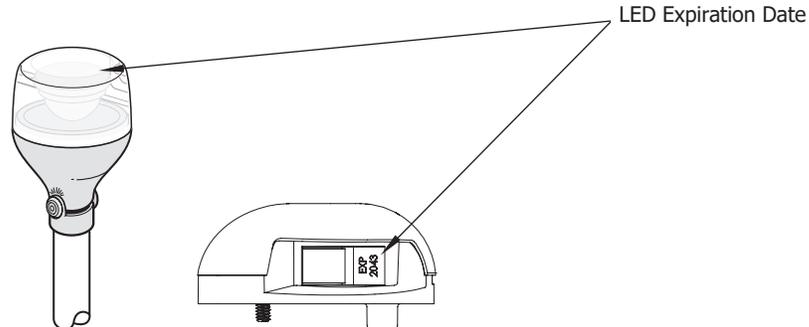


Fig. 18. Navigation lights

 CAUTION
Do not modify or replace the navigation lights with non-certified models, as this may compromise navigational safety and violate international maritime regulations.
Do not add lights that interfere with the navigation lights. Some lights, such as blue lights, may be illegal on a boat.

STEERING SYSTEM

The boat may be equipped with a mechanical or hydraulic steering system. Check steering for smooth operation before each use; if the steering is difficult or binding, do not operate the boat.

 WARNING
<p>Poor maintenance may result in loss of steering control and cause property damage and/or personal injury.</p> <p>Maintenance requirements change according to climate, frequency and use. Inspections are necessary at least every year and must be carried out by specialized marine mechanics.</p> <p>After the first 10 hours of use and periodically thereafter check that screws, bolts and nuts are tight and secure.</p> <p>Loose fasteners can result in steering system malfunction leading to injury or damage.</p>

 CAUTION
<p>The steering system is not self-centering. To assure complete control of the boat, always grip the steering wheel firmly.</p>

MECHANICAL STEERING

This system uses a push-pull cable connected between the steering wheel and the steering link connected to the boat's engine.

MAINTENANCE

It is recommended that the steering system be checked and serviced every 30 days if used in salt water, and every 90 days in fresh water.

Carry out the following maintenance operations:

- Check all end cable metal parts for corrosion and conduit abrasion.
- Check that all fastening items: bolts, screws and nuts of the helm and tilt mechanism (if applicable) are not loose.
- Replace any damaged parts that could compromise the steering system's integrity.
- Periodically wash the steering components with soap and water to remove any salt deposit.
- Remove excess grease and clean and protect exposed parts by applying a thin layer of new marine grease.

HYDRAULIC STEERING

This system uses a pump built into the steering wheel and hoses connected between the pump and a cylinder attached to the outboard steering link. Turning the wheel pumps hydraulic fluid to the cylinder to turn the engine.

Important Note:

This section provides general steering system maintenance guidelines. For specific procedures, adjustments, troubleshooting, hydraulic fluid specifications, and the correct filling and bleeding procedure of the hydraulic system, always refer to the manufacturer's steering system manual supplied with your boat.

MAINTENANCE

- Check that all fastening items: bolts, screws and nuts of the helm and tilt mechanism (if applicable) are not loose.
- Check cylinder fittings, seals and helm gaskets to prevent leaks. Replace them if necessary.
- To keep a suitable oil level in the tank, fill and bleed the system.
- Check the hose and the entire system wear, the nut and bolt tightening every six months and make sure that they are not damaged.
- Clean the steering system using water and non-abrasive soap.

FRESH WATER SYSTEM

Your boat may be equipped with a freshwater system (See Figures 19 and 20).

The system consists of a tank located below the deck, a water pump, and lines for filling, venting, and supplying freshwater to the swim platform shower.

To prevent a build-up of bacteria and impurities, empty, flush and refill the tank with fresh water at regular intervals.

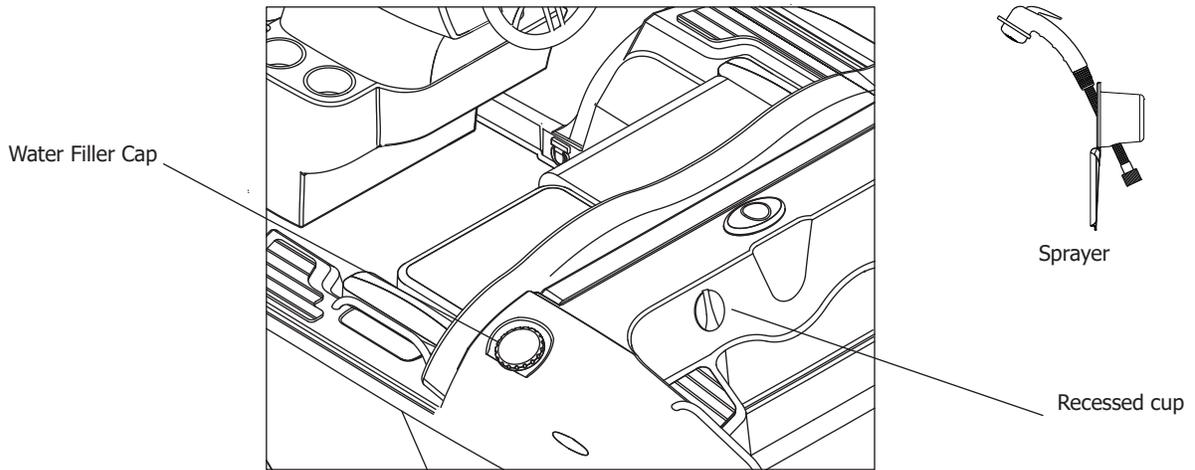


Figure 19. Fresh water system.

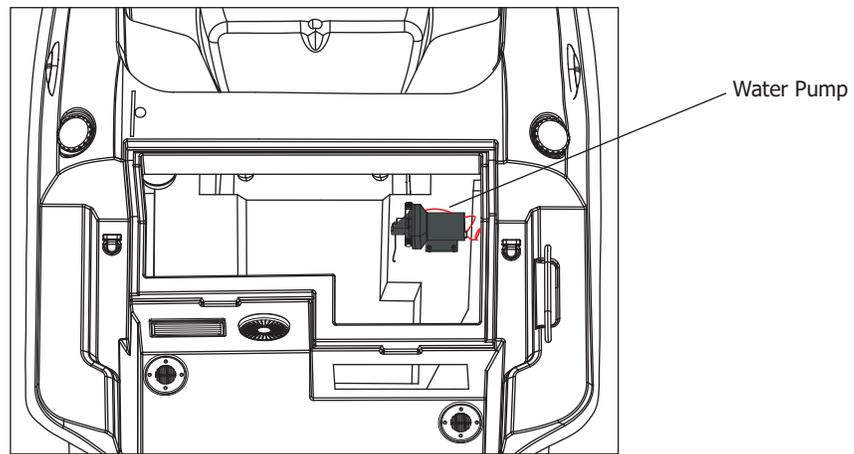


Figure 20. Example location of the water pump.

USING THE FRESHWATER SYSTEM

Before using the freshwater system, it is essential to ensure that it has been properly disinfected. Check with your dealer to determine whether this procedure has already been performed. If not, disinfect the system following the recommended procedure (see Tank Disinfection).

To operate the system, first activate the switch with the "faucet" symbol on the console switch panel. This will energize the freshwater system. Then, open the shower lid and remove the sprayer. Simply press the trigger to activate the pump and water will begin to flow from the sprayer.

Tank Filling

The water tank can be filled through the fill inlet located on the port side of your boat, marked with the word "Water." Fill the tank only with a source of clean, potable water.

Tank Disinfection

The following procedure is recommended for disinfecting the freshwater system:

- a. Flush entire system thoroughly by allowing potable water to flow through it.
- b. Drain system completely.
- c. Fill entire system with a chlorine solution having a strength of at least 100 ppm, and allow to stand for one hour (see AP TABLE 1).
- d. Drain chlorine solution from entire system. Consult with your local authority for proper disposal.
- e. Flush entire system thoroughly with potable water.
- f. Fill system with potable water.

AP TABLE 1 - Chlorine Concentrations

AMOUNT OF CHLORINE COMPOUND REQUIRED FOR 100 PPM SOLUTION			
SOLUTION (gal)	CLORINATED LIME (25%) (oz)	HIGH TEST CALCIUM HYPOCHLORITE 70% (oz)	LIQUID SODIUM HYPOCHLORITE 1% (qt)
5	0.3	0.1	0.1
10	0.6	0.2	0.2
15	0.9	0.3	0.3
20	1.2	0.4	0.4
30	1.8	0.6	0.6
50	3.0	1.0	1.0
100	6.0	2.0	2.0

Important Note: When you leave the boat unused for a longer period of time or store the boat over winter, the fresh water systems will build up bacteria and impurities, or will freeze in cold weather (See Winterizing). Before you leave or store your boat, empty the fresh water systems and tanks and flush them with fresh water.

MAINTENANCE

Normal pump maintenance is all that is needed: Checking and cleaning of the strainer, normal sanitizing and winterizing and occasionally checking all plumbing hardware and fittings for tightness. Lack of sanitizing is the number one reason for premature pump failure and poor performance over time. Lack of sanitizing will cause scale build-up on the diaphragm and valves, causing low flow and leak back [occasional pump cycling with no faucets open or tank filling up when hooked up to city water].

Shower Faucet Care

The shower faucet is protected with a marine-grade finish. To clean it, simply wipe it with a soft, damp cloth. NEVER use abrasive cleaners, steel wool, or chemical products (including alcohol or ammonia), as these will damage the finish and void the warranty.

Due to exposure to a salt-air environment, clean the faucet regularly with mild soap and apply carnauba wax to help preserve and extend the life of the finish.

WINTERIZING

If the boat will be stored in freezing conditions, it is important to properly drain and protect the freshwater system to prevent damage. The following simplified procedure applies to boats equipped with a freshwater tank, an electric water pump, and a single outlet (such as a transom shower).

Procedure

1. Drain the water tank. Open the faucet allowing the pump to operate until the tank is empty.
2. Using a pan to catch the remaining water, remove the plumbing at the pump's inlet/outlet ports. Turn the pump "ON", allowing it to operate until the water is expelled. Turn "OFF" power to the pump once the plumbing is emptied.
Do not reconnect the pump plumbing.
Make a note at tank filler as a reminder: "Plumbing is disconnected".

SKI TOW (IF APPLICABLE)

Your boat may be equipped with a ski tow, which is designed for use in watersports such as waterskiing, wakeboarding, and even for towing inflatable toys.

Important Note: All boats equipped with a ski tow post have a label attached to or near the post indicating the appropriate activities for its use. For example:

 WARNING
Tow point failure can result in serious injury or death
Do not attach inflatables, toys or vessels to this tow point.
Only use this watersports tow point for skiing, wakeboarding.

Watersports are great fun, but they involve inherent risks of injury or death. For information on proper waterski towing procedures, please contact your local boating school or yacht club.

 WARNING
When towing a skier or wakeboarder, only use a certified and verified tow rope.
Do not use any structural elements of the boat that are not designed for towing purposes, such as pop-up cleats, boarding handrails, or similar fittings.

TOWED INFLATABLE TOYS

Towed inflatables or tubes place significant strain on both the tow point and the tow rope—often much greater than the forces generated by water skis, kneeboards, or wakeboards. Tubes are frequently pulled partially underwater, increasing drag and resistance.

Avoid sharp turns, sudden acceleration, or towing in rough wave conditions. A violent acceleration immediately imposes an extreme shock load that can exceed the tow point's capacity limit; always accelerate slowly and gradually. Regularly inspect the condition of the Ski Tow, its mounting base, and all fasteners before each use.

 WARNING
Failure of the towing point may cause damage to the boat, serious injury to passengers, or death.
Do not tow more than one inflatable toy.
Do not use inflatable toy with more than 1 adult capacity.
Do not exceed a maximum towing speed of 20 mph (32 km/h).
The tow point is designed to tow a maximum of two adults. The combined weight of the riders and the inflatable must not exceed 375 lbs (170 kg).

ANCHORING - MOORING - TOWING

Make sure your boat is equipped with an appropriate anchor and line (see SAFETY EQUIPMENT). If you are not sure about selecting an anchor, consult your dealer for advice.

To secure boat to the dock use the rear left and right cleats and the bow step cleat. (see FEATURES & DECK PLANS).

Important Note: Some models have only one cleat in the center of the bow step.

It is the owner/operator's responsibility to ensure that mooring lines, towing lines, anchor chain(s), anchor lines and anchor(s) are adequate for the boat's intended use, i.e. the lines or chains do not exceed 80% of the breaking strength of the respective strong point.

Owners should also consider what action will be necessary when securing a tow line on board.



CAUTION

Anchoring and mooring lines should be attached to metal folding rings on bow.

Fasten boat securely. Failure to do so could result in loss of boat.



CAUTION

Always tow or be towed at a slow speed. Never exceed the hull speed of a displacement boat when being towed.

A tow line shall always be made fast in such a way that it can be released when under load.



WARNING

Keep clear of tow line. If tow line comes free or breaks, recoil can cause injury.

Do not tow with people in boat. They can fall overboard.



CAUTION

When towing inflatable behind another boat, use the folding rings on the sides of the bow (see FEATURES & DECK PLANS). Do not tow with the bow eye and hauling handle.

Use a double tow line when towing behind another boat. The second line acts as a backup in case one line breaks or comes undone.

Allow at least two boat lengths between boats.

Use line intended for towing, ask your dealer to select correct line for your application (Do not tow with three stranded nylon).

Towing line should have minimum breaking strength equal to 4 times weight boat being towed.

(continued on next page)



CAUTION

(continued from previous page)

Towing line should have minimum breaking strength equal to 4 times weight boat being towed.

Fasten boat securely and observe continuously. Do not attach tow line to fixtures not intended for towing, such as: fabric handles, pop up cleat and bow eye.

Avoid towing tender long distances. When traveling long distances or in rough weather stow boat on deck.

Empty inflatable before towing.

Do not use a tow line with signs of wear. Check tow lines for wear with each use.

Keep tow lines clear of propellers.

Keep hands and feet clear of tow lines. Do not hold tow line while towing.

Be prepared to cast loose or cut tow line if necessary.

Do not tow at high speeds or while planing.

BEACHING

Regularly check the surface of the fiberglass hull.

If the gel coat is damaged, please contact your Dealer for a repair service, if the defects are superficial scratches, see Gelcoat Repair.



CAUTION

Approach land slowly with extreme caution.

Avoid hazards below water surface such as rocks. Hazards can damage hull, tube, motor, or propeller.

Do not power boat onto land. You can damage hull or tube.

Do not drag boat across rocks, pavement, or sand. You can damage hull or tube.

DAVITS - CRADLES

Depending on your model you may have more than one lifting option, for specific information on your model visit www.walkerbay.com.



CAUTION

Use metal lifting rings identified in TYPICAL FEATURES AND DECK PLANS figure.

Elevate bow and open drain plug to prevent accumulation of water.

DAVITS:

- Do not attach lifting device to handles or features not intended for lifting.
- Do not lift boat with people onboard.

CRADLES:

- Use caution when positioning and securing boat on chocks, cradles, bunks and when trailering.
- Avoid positioning the boat in a manner where point loading may occur on lifting strakes.

- A bunk style trailer is recommended for towing.
- Bunks should be positioned on the outer side of the outer most lifting strakes.

- Excessive point loading or failure to adequately support hull may damage the hull. Damage resulting from not properly supporting the hull or motor is not covered under warranty.

TRANSPORTING BY ROAD



WARNING

Risk of automobile accident, injury or loss of life in case of loss of control of trailer or boat while driving.

ROOF RACK

- Do not attempt to use a roof rack to transport the boat.

TRAILER

- Read Owner's Manual provided with trailer.
- Use a BUNK trailer. Do not use a ROLLER trailer. Rollers do not provide adequate support for hull.
- Use a trailer suitable for the type of craft and its mass.
- Secure boat to trailer and stop frequently to check boat is securely fastened while driving.
- Do not exceed maximum capacity of trailer.
- Do not trailer boat with objects inside hull.

Check with trailer manufacturer if weight and shape of boat is sufficient for control at driving speeds.

5. TECHNICAL INFORMATION

TECHNICAL SPECIFICATIONS

For specifications such as: dimensions, recommended and maximum power, maximum load, as well as the capacities of the fuel and fresh water tanks, etc., see the attached sheet "SPECIFICATIONS AND MAXIMUM CAPACITIES".

ELECTRICAL DIAGRAM

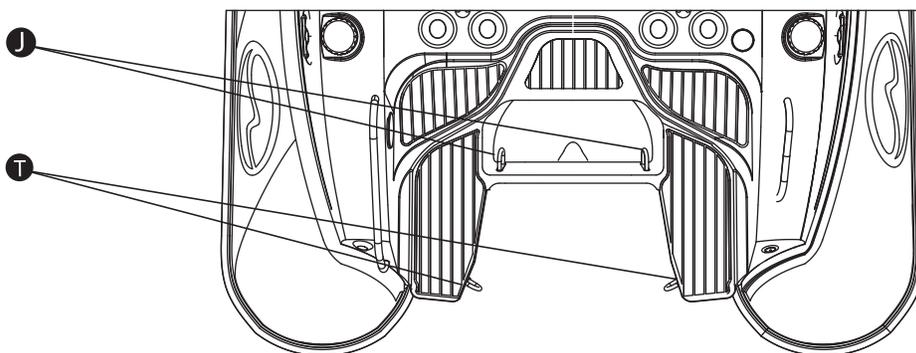
For modifications, repairs or maintenance to the electrical power system of the boat, see the attached sheet "Electrical Diagram".



WARNING

All modifications, repairs, or planned maintenance must be performed exclusively by authorized marine technicians.

YOUR BOAT TYPICAL FEATURES AND DECK PLANS



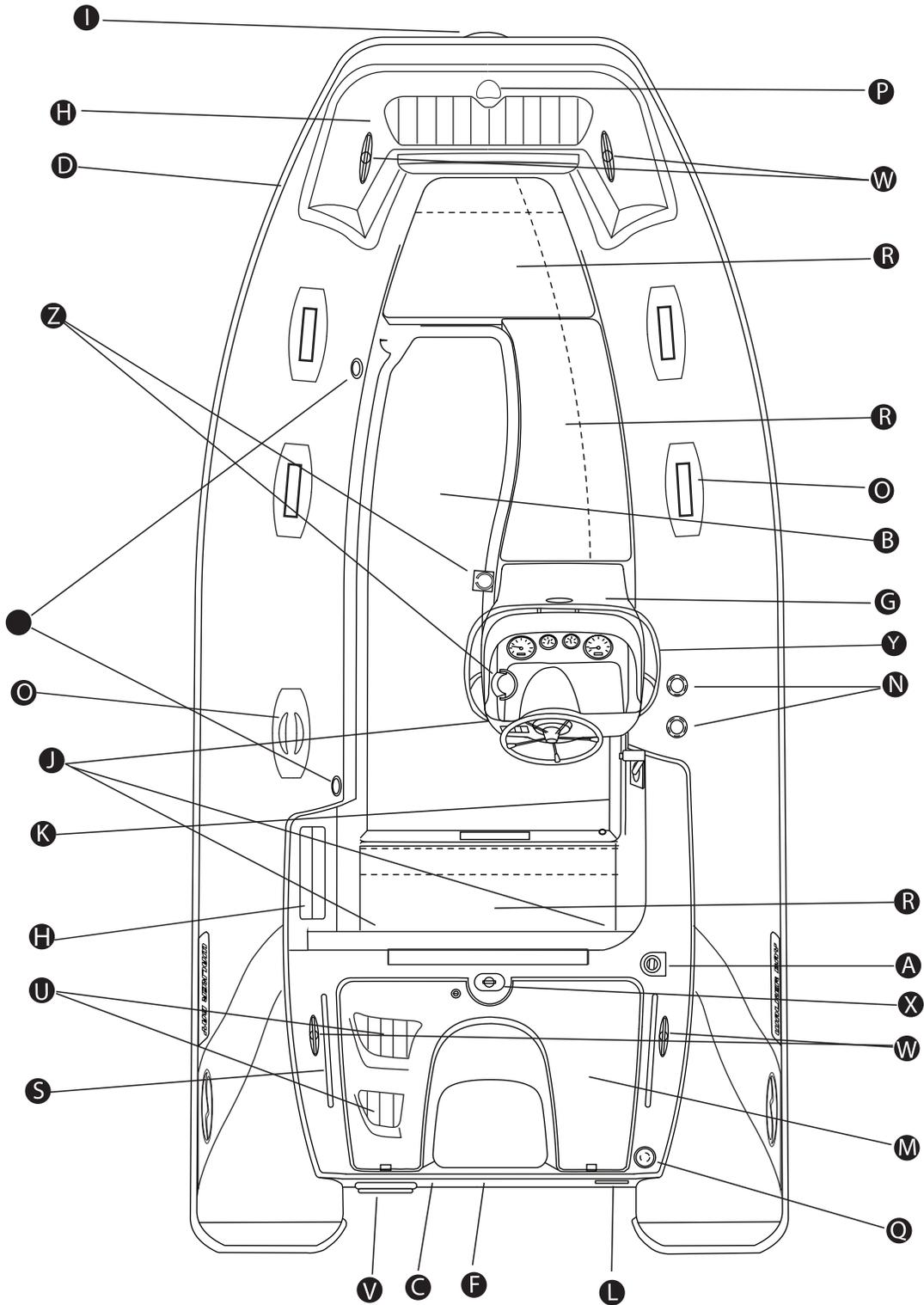
J - Davit Lifting Points

T - U-Bolts for Tubing and Trailer

Generation 12 shown.

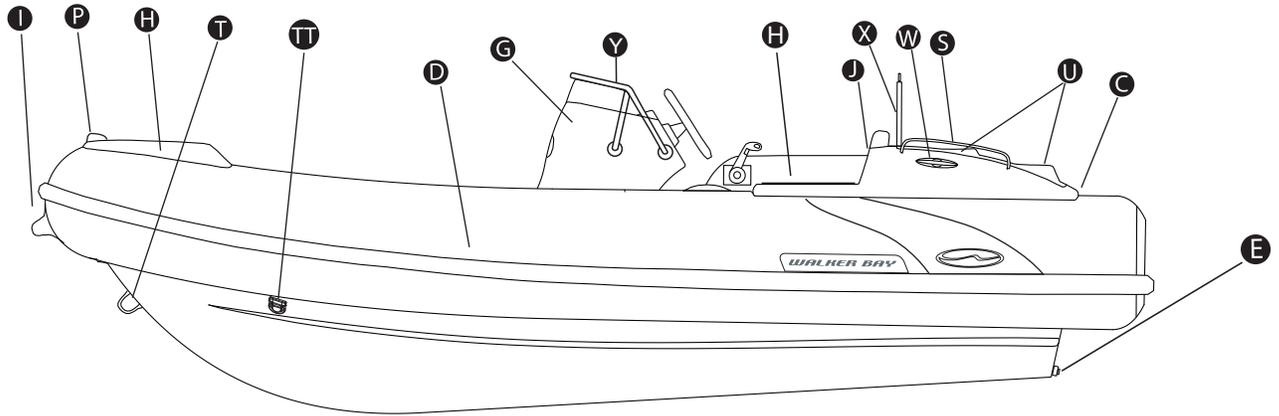
Actual features and deck plan will vary by model and year of boat and are subject to change without notice.

YOUR BOAT - TYPICAL FEATURES AND DECK PLANS



Generation 450 shown.
 Actual features and deck plan will vary by model and year of boat and are subject to change without notice.

YOUR BOAT - TYPICAL FEATURES AND DECK PLANS



Generation 450 shown.

Actual features and deck plan will vary by model and year of boat and are subject to change without notice.

- | | |
|-------------------------------|---|
| A - Fuel Tank Cap | N - Valves (3-5 depending on model) |
| B - Deck Floor | O - Handles (not for tying or davits) |
| C - Transom | P - Navigation Lights (red and green) |
| D - Rub Rail | Q - Navigation Light Mount (for 360° white light) |
| E - Drain Plug | R - Seats, Cushions and Under-seat Storage Compartments |
| F - Motor Mount | S - Stainless Steel Hand Rails |
| G - Console | T - Bow Eye for Trailer |
| H - Step | TT - Folding Rings for Towing |
| I - Hauling Handle | U - Stern Steps |
| J - Davit Lifting Points | V - Stern Boarding Ladder |
| *K - Builder's Plate | W - Pop-up Cleats |
| L - Hull ID (WIN) Number | X - Retractable Ski Post (Ski Tow) |
| M - Stern Storage Compartment | Y - Console Handrail |
| | Z - Permanent and Retractable Cup Holders |

*Replace capacity and warning labels if they become damaged, faded or illegible. For reordering and replacement of labels contact Walker Bay® Customer Service.

MATERIALS

Walker Bay® uses materials and fabric from the world's most respected manufacturers. We take extra care in our construction to create a line of boats you can depend on.

ORCA® CSM Walker Bay uses the finest ORCA® CSM material available. Four layers of calendared sheets offer guaranteed air-tightness and optimal adhesion of rubbers. This combination of materials provides improved resistance against weathering, chemicals and abrasions. The tubes are bonded using a three-layer gluing process with all seams internally and externally butted for maximum reliability.

PREMIUM FIBERGLASS - Walker Bay® uses top quality FRP (Fiber Reinforced Polymer) to build a hull that is both stiff and strong in tension and compression, which is important to the performance of the boat. A fine glossy gelcoat not only creates a glossy, high-lustre, ultra-smooth finish, but provides maximum durability as well.

6. SERVICE AND MAINTENANCE

MAINTENANCE

FRP (FIBERGLASS) HULL AND GELCOAT - Use a mild boat cleaning agent or cleaner mixed with fresh water. Make sure the cleaning agent you use does not contain ammonia or chlorine. Ammonia or abrasive type cleaners will dull and discolor the surface of the gelcoat.

Use a fiberglass wax that is capable of filling gelcoat pores, and screening out harmful ultraviolet rays that may cause the gelcoat to fade. Wax at least twice every season.

On heavily used areas such as the deck, fiberglass surfaces may be severely scratched by dirt, sand or footwear. To prevent surface damages, keep it as clean as possible.

At the start and the end of your boating season check the condition of fasteners, screws, bolts, nuts, rivets and other fittings. Tighten or replace if necessary. For information contact Walker Bay® customer service.

TUBES - After use, rinse boat with mild liquid soap and fresh water, rinse away sand and debris.

Apply 303 Aerospace Protectant at least once per season to protect tube from UV (ultra violet) rays. Walker Bay® recommends 303 because it is free of silicone oils, waxes, glycerin, or petroleum distillates unlike many other protectants. 303 leaves a like-new, non-oily, anti-static finish that does not attract dust. 303 can be purchased through the manufacturer's website: www.303products.com

VALVES - Tighten air valves several times per season. Loose valves may result in air leaks or loss of pressure in the tube. Tighten valves with the tool provided in the repair kit.

Valve replacement should be done by a trained professional. Contact your dealer to replace valves.

METAL - To prevent corrosion, Walker Bay uses only stainless steel and anodized or powder coated aluminum metal parts.

Stainless steel nuts and bolts may show signs of corrosion. This is due to metallic residue from contact with non-stainless metal items like screwdrivers or wrenches.

Rinse metal parts with fresh water when cleaning the rest of the boat.

To replace metal parts or add components containing metal, use stainless steel and anodized or powder coated aluminum to prevent corrosion.

STEERING WHEEL.- Periodically clean the steering wheel to remove any salt deposit: rinse with fresh water and dry with a clean cloth. Do not use any aggressive soap or alcohol based cleaner or solvent that may damage the steering wheel. Use specific cleaning products on leather finished steering wheels (if applicable); automotive leather seats cleaners and cloths are the most indicated.



CAUTION

Do not use oily or abrasive substances.

Do not use products that contain phosphates, chlorine, solvents, non-biodegradable or petroleum based products.

Do not use Armor All® automotive cleaning agent and preservative on tube.

Use household cleaners sparingly and do not discharge them into waterways.

Avoid contact of the tubes' fabric with hazardous or harmful materials such as: battery acid, oil, gasoline, vinyl preserving agents, or alcohol based cleaning solutions, waxes, or solvents.

Avoid contact of sharp or abrasive objects with the tube. They can puncture the tube.

Do not mix cleaners and work in a well ventilated area.

REPAIRS

For repairs, parts, or service contact your local dealer or Walker Bay® customer service at:
email: info@walkerbay.com

GELCOAT REPAIR:

Gelcoat may fade or chalk with time or be scratched with use. Here are instructions to repair gelcoat.

Surface scratches can be buffed out of gelcoat with polishing compound. Deep scratches must be filled. If the gelcoat surrounding a scratch is in good condition then a gelcoat paste from a marine supply store is recommended for repair.

Gelcoat repair kits include a small quantity of gelcoat paste, hardener, a selection of pigments, mixing sticks and sealing film. We recommend a flexible plastic spreader. You will need sheets of 150, 220, 400, and 600 grit wet-or-dry sandpaper.

To repair multiple scratches we recommend a small bottle of styrene. Wiping the scratch with styrene prior to filling it partially reactivates the old gelcoat resulting in a chemical bond between the old and new gelcoat.

Preparing the Scratch

Do not repair a scratch by simply painting over it with gelcoat. For a permanent repair, draw the corner of a scraper or screwdriver down the scratch to open it into a vee and wipe the open scratch with styrene to reactivate the old gelcoat.

Catalyzing

The hardener for gelcoat is the same for any polyester resin: methyl ethyl ketone peroxide, or MEKP. Gelcoat resin usually requires 1 to 2 percent of hardener by volume (Follow the gelcoat manufacturer's instructions). The mix should not "kick" or start to harden in less than 30 minutes. Ideal hardening time is approximately two hours. Use caution when adding hardener, too little hardener is better than too much. Stir and mix the hardener thoroughly.

Spreading Gelcoat Paste

Work the gelcoat paste into the scratch with a flexible plastic spreader. Polyester resin shrinks slightly as it cures so allow the paste to bulge a little behind the spreader tool when applying. You will sand the patch when it has dried. Scrape or clean any excess paste outside the patch area.

Covering the Repair

Gelcoat will not fully cure in air. To seal the surface of a scratch repair cover it with a sheet of plastic film. The repair kit should include sealing film, if not a section of "Ziploc" bag works well as it tends to remain smooth without adhering to the gelcoat. Tape one edge of the plastic to the surface just beyond the repair, then smooth the plastic onto the gelcoat and tape down the remaining sides.

Sanding and Polishing

After 24 hours, peel away the plastic. A 6" length of 1" x 2" wood makes a convenient sanding block. Wrap the block with a quarter sheet of 150 grit paper. Use the end of the block to confine your sanding to the new gelcoat. Use short strokes. Take care to only sand the patch and not the surrounding surface. Never do this initial sanding without a block backing the paper.

When the new gelcoat is flush or level with the old gelcoat, put 220 grit wet-or-dry sandpaper on the wood block and wet sand the repair, this time with your block flat. Use a circular motion and keep water on the sanding area. Feather the repair into the old gelcoat until you cannot detect a ridge with your fingertips.

Switch to 400 grit wet-or-dry paper without the wood sanding block. Wet sand the surface until the repair area has a uniform appearance. Repeat this step with 600 grit wet-or dry sand paper.

Dry the area and use rubbing compound to give the gelcoat a high gloss. Buff the repaired area in a circular motion using heavy pressure to start. Progressively reduce the pressure until the surface becomes glassy. If the gelcoat shows swirl marks, buff them out with a fine finishing compound.

Finish by applying a fresh coat of wax to the area.



CAUTION

In the event of more serious hull damage such as a hole or crack do not use the boat. Contact your dealer.

TUBE

SMALL TEARS, CUTS, PUNCTURES

Tears, leaks, or punctures less than 0.5 inches or 1cm can be repaired with the repair kit provided.

1. Prepare a round patch no less than 3 inches or 7 centimeters in diameter.
2. Clean the patch and the area surrounding the leak with Isopropyl Alcohol. **IMPORTANT:** patch and tube must be clean or the patch may not adhere properly.
3. Mildly rough the patch surface area of the tube using a small square of sand paper.
4. To contain excess glue, use masking tape to tape off the area around the patch. Excess glue will discolor.
5. Apply 3 thin, even coats of repair kit adhesive to both the surface of the tube and the patch. Wait 5 minutes between each coat.
6. After the 3rd coat, wait 10-15 minutes before applying the patch. Use a hand roller to apply the patch evenly. Pay special attention to the edges.
7. Wait 24 hours before re-inflating tube. Inflate air chamber(s) to working pressure and check water-tightness and/or air-tightness before using.

FINDING SMALL LEAKS

1. Mix a solution of 4 cups of fresh water and 1 cup of mild non-anti-bacterial liquid soap. Do not use a spray bottle containing any household cleaners.
2. Inflate tube approximately to working pressure.
3. Spray the soapy solution over suspected area of leak.
4. Look for air bubbles. Bubbles around an air valve indicate it is probably loose. Use the tool included in the repair kit and gently tighten valve. Bubbles on the surface of the tube indicate a puncture or leak.
5. Repair the tube by following the procedure for SMALL TEARS - CUTS - PUNCTURES or contact dealer nearest you.

LARGE REPAIRS TO SKIN - SEAMS BULKHEADS - TRANSOM



CAUTION

For large tears or repairs to seams, bulkheads or transom it is recommended that the boat be returned to your dealer for repair by a trained professional. Contact Walker Bay® customer service for the location of the dealer nearest you.

STORAGE

Rinse boat with mild soap and fresh water after use. Rinse away sand or debris.

Dry boat before storing to prevent accumulation of mould.

Store boat in a cool and dry place.



CAUTION

Risk of fire. Do not store fuel, fuel tanks, fuel lines or any fuel related components in direct sunlight. Read Owner's Manual provided with your outboard motor for storage information.

Avoid storing boat in water when not in use to prevent contact with hazardous chemicals due to pollution, abrasion from foreign objects and accumulation of marine growth.

Use a boat cover. Avoid unnecessary exposure of the tubes to direct sunlight. If a cover is not provided with your boat, accessory covers are available. For information contact Walker Bay® customer service.

Do not store boat where it may come in contact with sharp or abrasive objects.

Do not store boat where rodents may have access, they can chew through the tube's fabric creating a leak.

Do not place heavy objects on boat in storage.

Tie boat down if you keep it in a potentially windy area.

7. RESPECT TO ENVIRONMENT

PROTECT THE ENVIRONMENT - BE A CLEAN BOATER

As a boat owner you must be aware of the local environmental laws, and respect codes of good practice. Familiarize yourself with the international regulations on the prevention of marine pollution (MARPOL) and comply with these regulations.

The practice of clean boating and good environmental stewardship make sense economically, for our family's health and safety, for the environment, and for the future of recreational boating. Clean Boating includes all aspects of boat maintenance, operation, and housekeeping. Care must be taken during cleaning, fueling, motoring, pumpout, and trash disposal to minimize potential effects on the aquatic environment.

Cleaning Your Boat:

When washing a boat's deck and hull surface, use products that do not contain toxic ingredients. See MAINTENANCE for guidelines on how cleaning agents should be chosen.

Fueling and Bilge Maintenance:

Take precautions not to overfill your fuel tank. If you overflow onto the boat or dock, wipe up the spill with a rag; do not hose it into the water. If you do spill fuel or oil into the water, do not disperse it with detergent or soap! That only sends the problem down to the seafloor where it becomes more toxic and more difficult to clean up. If the spill is large or if it discolors the surface of the water, you must report it to the local authorities. Failure to do so is illegal and can cost you civil penalties and/or criminal sanctions.

Exhaust Emission:

Increased exhaust emissions (hydrocarbons) cause water and air pollution. To achieve the highest efficiency, have the engine properly tuned and the hull cleaned regularly. See the engine manufacturer's manual for engine tuning directions.

Clean Motoring:

If you become grounded, do not attempt to motor your way out. This could cause serious damage not only to your motor and propellers, but also to the seafloor and local marine organisms. If you sight a marine mammal such as a manatee, dolphin or whale, slow down and keep a safe distance of at least 100 yards. It is illegal to feed, harass, molest or injure a marine mammal.

Clean Trash Discharge:

Stow all loose items, plastic bags, drink cans, and other articles properly so they do not blow overboard. Never discard your garbage overboard. Whatever you take aboard, bring back.

Black Water Discharge:

Do not empty septic or black water tank into the water, near shore or in any prohibited zone, use the harbor or marina pump-out facilities to empty holding tank.

8. WARRANTY

WALKER BAY® CONSOLE RIB LIMITED WARRANTY

1. Nature and Extent of Warranty: Walker Bay Boats, LLC. ("Manufacturer") warrants to the original retail purchaser ("Purchaser") that any Walker Bay® console RIB or component ("Product") shall be free from defects in workmanship and/or materials for a period of one (1) year after the date of original retail purchase. In the event of such a defect, Manufacturer will, at its option, repair or replace the defective Product or part thereof without charge (Excluding shipping expenses) subject to the following terms.
2. Extended Warranty. Purchaser upon receipt by Manufacturer of a completed Warranty Registration Claim online (information provided with boat) within 30 days of the date of purchase will be entitled to the following specific warranties, subject to the exclusions, conditions and terms of this Limited Warranty:
 - 2.1. Boat FRP hull is covered under warranty for a period of one (1) year from date of original retail purchase.
 - 2.2. ORCA CSM® fabric and seams are covered against delaminating under warranty for a period of ten (10) years from date of original retail purchase, prorated as in the Walker Bay® Console RIB Limited Warranty Dealer Service Guide.
3. Exclusions. In the case of Walker Bay® Console RIB boats this warranty does NOT cover:
 - 3.1. Normal color fading, or discoloration;
 - 3.2. Normal wear and tear;
 - 3.3. Abrasions, tears or punctures;
 - 3.4. Damage caused by abuse, neglect or accident;
 - 3.5. Damage caused by improper assembly, service or maintenance in accordance with the Owner's Safety Manual provided with each Walker Bay® Console RIB;
 - 3.6. Damage caused by the installation or use of an accessory or parts not manufactured or sold by Walker Bay;
 - 3.7. Damage caused by alteration or removal of parts;
 - 3.8. Incorrect towing, lifting and/or storage on chocks;
 - 3.9. Expenses related to hauling out, launch, towing, storage, loan payments, loss of time, loss of income, or any other type of incidental or consequential damages;
 - 3.10. Shipping costs. All expenses to an authorized Walker Bay dealer is the purchaser's responsibility.
4. Non-recreational Use. If any Walker Bay® Console RIB is purchased or used for racing, rental or other commercial purposes, or for/by a corporation, partnership, company, organization or association including without limitation camps, resorts, schools, or the like, then the term of this Limited Warranty is limited to a period of ninety (90) days after the date of original retail purchase, and Section 2 (Extended Warranty) shall not apply.
5. Life of Warranty Coverage. The repair and/or replacement of parts or the performance of service under this warranty does not extend the life of this warranty beyond its original expiration date.
6. Transfer of Unexpired Warranty Coverage. Unexpired warranty coverage can only be transferred to a subsequent purchaser upon proper re-registration of the product through the Manufacturer using the Manufacturer's Statement of Origin provided with each Walker Bay® Console RIB (Within the first year starting from the date of original purchase).
7. Conditions to Be Met.
 - 7.1. Warranty coverage becomes available upon proper registration of the Product by the authorized dealer or purchaser.
 - 7.2. Routine and timely maintenance outlined in the Owner's Manual must be performed in order to maintain warranty coverage.
8. How to Obtain Warranty Coverage. To obtain warranty coverage, claims shall be made by delivering the Product for inspection to a Walker Bay® dealer authorized to service the Product. The Purchaser must provide the Dealer with the boat or claimed defective part and proof of purchase (Bill of Sale) within 30 days of occurrence of the claimed defect. Purchaser must provide the Manufacturer with reasonable opportunity and access to the Product for repair and/or service.
9. Manufacturer's Obligation. The sole obligation of the Manufacturer under this warranty is limited to, at its option, repairing a defective part, replacing such part or parts with new or Walker Bay® certified re-manufactured parts, or refunding the purchase price of the Walker Bay® Product. Manufacturer's sole obligation against fabric delaminating under the prorated Walker Bay® Console RIB Limited Warranty is the replacement of the boat and/or tube at the percentages identified on the prorated Walker Bay® Console RIB Limited Warranty Dealer Service Guide.

APPENDIX: AIS DECONTAMINATION - NORTH AMERICA

According to ABYC T-32 Design and Construction in Consideration of Aquatic Invasive Species

TABLE 1 - Decontamination Criteria Based on UMPS III Table 3

The table is a summary of scientific research indicating the lethal water temperature at point of contact and duration for decontamination. Information is grouped by the location of the boat that is targeted and the life form of Dreissenid mussel targeted (e.g., adult mussel or veliger). Please refer to the Student Training Curriculum for Watercraft Inspectors and Decontaminators to Prevent and Contain the Spread of Aquatic Invasive Species in the USA for complete step by step procedures.

	BOAT PART/ LOCATION	WATER TEMPERATUR	DURATION* (SEC)	TYPE OF APPLICATION	TARGET LIFE SATGE
EXTERIOR	Hull	140°F	10	High pressure spray ¹	Adult
	Trailer	140°F	70	Low pressure spray ²	Adult
	PFDS, anchor, paddle	140°F	10	Low pressure spray	Adult or Veliger
PROPULSION SYSTEM	Gimbal	140°F	132	Low pressure spray	Adult
	Engine	140°F ^{4,5}	See note	Flush	Veliger
INTERIOR	Ballast tanks	120°F ⁴	130	<i>Low risk</i> - Flush ³ <i>High risk</i> - Fill and flush	Veliger
	Live well/bait well	120°F	130	Low pressure spray or flush	Veliger
	Bilge	120°F	130	Flush or low pressure spray	Veliger

*The times listed are the minimum times necessary to achieve mortality.

¹High pressure = 3000 psi.

²Low pressure = using the pressure from the decontamination unit with no nozzle, not to exceed 60 psi (essentially a garden hose flow).

³Flush = adding water to a compartment of a boat to treat or force the water out.

⁴These temperatures denote the exit temperature (i.e., temperature of water exiting the boat not exiting the wand or flush attachment).

⁵When flushing engines with a dedicated connection (not muffs) the pressure should be limited to less than 60 psi to prevent internal engine damage. The maximum input temperature during flushing should not exceed 140°F.

NOTE: Engine flushing relies on the exit temperature as a guideline for decontamination duration.

Aquatic Invasive Species

Aquatic invasive species (AIS) are plants and animals that occur in waters in which they are not native and whose introduction causes or is likely to cause economic or environmental damage or harm to human health. AIS have a negative impact on the waterway, its native species, and recreational and commercial uses of the waterway.

As responsible boaters and citizens, each boat owner should do their part to prevent the spread of these aquatic hitchhikers. In many cases, it is also required by law. Check local regulations for any waterway where you will boat.

After each boating trip, follow these three simple steps before you leave the water access to stop the spread of AIS: Clean, Drain, and Dry. This is the boater's way to help protect the environment from the damage that AIS can cause.



STOP AQUATIC HITCHHIKERS!™

Be A Good Steward. Clean. Drain. Dry.
StopAquaticHitchhikers.org

Clean

Inspect and remove all aquatic plants, animals, mud, and debris from the boat, engine, trailer, anchor, and any watersports equipment.

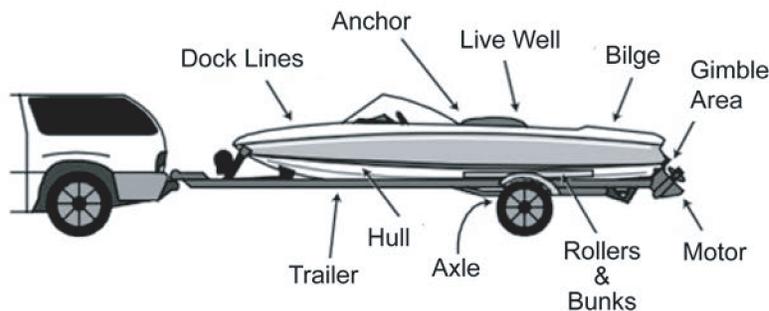
Rinse, scrub or wash, as appropriate, away from storm drains, ditches, or waterways.
Rinse watercraft, trailer, and equipment with hot water, when possible.
Flush motor according to owner's manual.

Drain

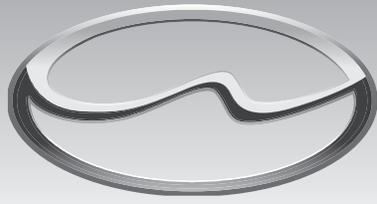
Completely drain all water from the boat and its compartments, including but not limited to the bilge, wells, lockers, ballast tanks or bags, bait containers, engines, and outdrives.

Dry

Allow the boat to completely dry before visiting any other bodies of water.



NOTE: Some localities may require inspection or decontamination before and/or after launching. Check state and local laws and regulations for requirements prior to traveling to go boating.



WALKER BAY